



Public Document Pack

Cambridge City Council

Planning Committee

Committee Members: Councillors Stuart (Chair), Blencowe (Vice-Chair), Brown, Dryden, Hipkin, Marchant-Daisley, Saunders and Tunnacliffe

Alternates: Councillors Herbert and Tucker

Published & Despatched: Tuesday, 26 February 2013

Date: Wednesday, 6 March 2013
Time: 9.30 am
Venue: Committee Room 1 & 2 - Guildhall
Contact: James Goddard

AGENDA

1 Apologies

2 Declarations of Interest

Members are asked to declare at this stage any interests, which they may have in any of the following items on the agenda. If any member is unsure whether or not they should declare an interest on a particular matter, they are requested to seek advice from the Head of Legal Services before the meeting.

3 Minutes

To confirm the minutes of the meeting held on 6 February 2013. (*Pages 1 - 14*)

Planning Items

4 Planning Applications

- 4a 12/1556/FUL - 32-38 Station Road (*Pages 25 - 126*)
- 4b 12/1553/CAC - 32-38 Station Road (*Pages 127 - 158*)
- 4c 12/1512/FUL - 1 Benson Place (*Pages 159 - 188*)
- 4d 12/1513/CAC - 1 Benson Place (*Pages 189 - 200*)
- 4e 12/1537/FUL - Land R/O 231-247 Milton Road (*Pages 201 - 246*)
- 4f 13/0034/REM - West Cambridge, Madingley Road (*Pages 247 - 272*)

5 General Items

5a EC Language School, Gibson House, 57-61 Burleigh Street (*Pages 273 - 276*)

Meeting Information

Location The meeting is in the Guildhall on the Market Square (CB2 3QJ).

Between 9 a.m. and 5 p.m. the building is accessible via Peas Hill, Guildhall Street and the Market Square entrances.

After 5 p.m. access is via the Peas Hill entrance.

All the meeting rooms (Committee Room 1, Committee 2 and the Council Chamber) are on the first floor, and are accessible via lifts or stairs.

**Local
Government
(Access to
Information)
Act 1985**

Under Section 100D of the Local Government Act 1972, the following are “background papers” for each of the above reports on planning applications:

1. The planning application and plans;
2. Any explanatory or accompanying letter or document from the applicant;
3. Comments of Council departments on the application;
4. Comments or representations by third parties on the application as referred to in the report plus any additional comments received before the meeting at which the application is considered; unless (in each case) the document discloses “exempt or confidential information”
5. Any Structure Plan, Local Plan or Council Policy Document referred to in individual reports.

These papers may be inspected by contacting Patsy Dell (01223 457103) in the Planning Department.

**Development
Control
Forum**

Meetings of the Development Control Forum are scheduled for a week after the meetings of Planning Committee if required

**Public
Participation**

Some meetings may have parts, which will be closed to the public, but the reasons for excluding the press and public will be given.

Members of the public who want to speak about an application on the agenda for this meeting may do so, if they

have submitted a written representation within the consultation period relating to the application and notified the Committee Manager that they wish to speak by **12.00 noon on the day before** the meeting.

Public speakers will not be allowed to circulate any additional written information to their speaking notes or any other drawings or other visual material in support of their case that has not been verified by officers and that is not already on public file.

For further information on speaking at committee please contact Democratic Services on 01223 457013 or democratic.services@cambridge.gov.uk.

Further information is available at

<http://www.cambridge.gov.uk/public/docs/Having%20your%20say%20at%20meetings.pdf>

The Chair will adopt the principles of the public speaking scheme regarding planning applications for general items, enforcement items and tree items.

Cambridge City Council would value your assistance in improving the public speaking process of committee meetings. If you have any feedback please contact Democratic Services on 01223 457013 or democratic.services@cambridge.gov.uk

Representations on Planning Applications

Public representations on a planning application should be made in writing (by e-mail or letter, in both cases stating your full postal address), within the deadline set for comments on that application. You are therefore strongly urged to submit your representations within this deadline.

The submission of late information after the officer's report has been published is to be avoided.

A written representation submitted to the Environment Department by a member of the public after publication of the officer's report will only be considered if it is from someone who has already made written representations in time for inclusion within the officer's report. Any public representation received by the Department after 12 noon two

business days before the relevant Committee meeting (e.g by 12.00 noon on Monday before a Wednesday meeting; by 12.00 noon on Tuesday before a Thursday meeting) will not be considered.

The same deadline will also apply to the receipt by the Department of additional information submitted by an applicant or an agent in connection with the relevant item on the Committee agenda (including letters, e-mails, reports, drawings and all other visual material), unless specifically requested by planning officers to help decision-making.

**Filming,
recording
and
photography**

The Council is committed to being open and transparent in the way it conducts its decision making. Recording is permitted at council meetings which are open to the public. The Council understands that some members of the public attending its meetings may not wish to be recorded. The Chair of the meeting will facilitate by ensuring that any such request not to be recorded is respected by those doing the recording.

Full details of the City Council's protocol on audio/visual recording and photography at meetings can be accessed via:

<http://democracy.cambridge.gov.uk/ecSDDisplay.aspx?NAME=SD1057&ID=1057&RPID=33371389&sch=doc&cat=13203&path=13020%2c13203>.

Fire Alarm

In the event of the fire alarm sounding please follow the instructions of Cambridge City Council staff.

**Facilities for
disabled
people**

Level access to the Guildhall via the Peas Hill entrance.

A loop system is available in Committee Room 1, Committee Room 2 and the Council Chamber.

Accessible toilets are available on the ground and first floor.

Meeting papers are available in large print and other formats on request.

For further assistance please contact Democratic Services on 01223 457013 or democratic.services@cambridge.gov.uk.

Queries on reports

If you have a question or query regarding a committee report please contact the officer listed at the end of relevant report or Democratic Services on 01223 457013 or democratic.services@cambridge.gov.uk.

General Information

Information regarding committees, councilors and the democratic process is available at <http://democracy.cambridge.gov.uk>.

PLANNING COMMITTEE

6 February 2013

9.30 am - 1.00 pm

Present:

Planning Committee Members: Councillors Stuart (Chair), Blencowe (Vice-Chair), Brown, Hipkin, Saunders and Tunnacliffe

Other Members in Attendance:

County Councillor Wilkins

Officers:

Head of Planning Services: Patsy Dell
 City Development Manager: Sarah Dyer
 Principal Planning Officer: Toby Williams
 Interim Arboricultural Officer: Joanna Davies
 Planning Officer: Sav Patel
 Legal Advisor: Cara de la Mare
 Legal Advisor: Victoria Watts
 Committee Manager: James Goddard

Other Officers in Attendance:

Head of Arts & Recreation: Debbie Kaye
 Principal Planning Officer: Tony Collins

FOR THE INFORMATION OF THE COUNCIL**13/7/Plan Apologies**

Apologies were received from Councillors Dryden, Herbert and Marchant-Daisley.

13/8/Plan Declarations of Interest

Name	ITEM	Interest
Councillor Blencowe	13/10/PLANa	Personal: Chair of YMCA Football Club Associated with Cambridge City Football Club

Councillor Saunders	13/10/PLANa	Personal: Member of Cambridge Past, Present & Future
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13/9/Plan Minutes

The minutes of the 9 January 2013 meeting were approved and signed as a correct record subject to the following amendments (shown as bold or struck out text):

13/5/PLANa Motor Ace Car Sales Site

Resolved (by 7 votes to 1) to reject the officer recommendation to refuse the application.

Resolved (unanimously) to approve the application contrary to the officer recommendations **for the following reasons for approval**, subject to **the completion of a section 106 agreement to secure the obligations as set out in the report and subject to** conditions, which will be reported to Members **for agreement** at February's Committee.

~~Reasons and~~ Conditions to be reported to Members at February's Committee.

Reasons for Approval:

Subject to conditions to be agreed and to the completion of a section 106 planning obligation, the proposed development is considered to conform to the Development Plan as a whole, particularly the following policies:

Cambridge Local Plan (2006): 3/4, 3/6, 3/7, 3/8, 3/12, 5/5, 5/14, and 10/1.

The decision has been made having regard to all other material considerations, none of which were considered to have been of such significance as to justify not granting planning permission.

The Committee took the view that the development would not have an unacceptable adverse impact on the residential amenity of the occupants of Parkers Terrace and the development takes appropriate consideration of the need to ensure coordinated development, and would not sufficiently prejudice the co-ordinated and successful redevelopment of the proposal site 7.01.

13/10/Plan Planning Applications

13/10/Plana 12/1211/FUL - Cambridge City Football Club, Milton Road

The Committee received an application for full planning permission.

The application sought approval for proposed residential development of 138 dwellings incorporating affordable housing, open space and landscaping, car and cycle parking and access roads and demolition of existing buildings and structures

Mr Lainchbury (Applicant's Agent) addressed the Committee in support of the application.

The Committee received a representation in support of the application from Mr Bond speaking as a member of Old Chesterton Residents Association, Chesterton Community Association and Fields in Trust.

Councillor Wilkins (West Chesterton Ward County Councillor) addressed the Committee about the application.

The representation covered the following issues:

- (i) Spoke in favour of the application, but had some reservations.
- (ii) Expressed concern regarding:
 - Permeability of site.
 - Development of the site. Referred to Councillor Boyce's comments regarding the building and access road made when the application last came to Committee. Councillor Wilkins asked for planning conditions to manage considerate construction if the application was approved.

The Committee:

Resolved (by 5 votes to 1) to accept the officer recommendation to approve planning permission as per the agenda, subject to a S106 agreement and subject to the proposed conditions as amended and set out on the amendment sheet:

- (i) Condition 20 (archaeology): Removed.
- (ii) Condition 25: The new streets to be constructed within the

development shall be constructed and permanently maintained to an adoptable standard.

Reason: To ensure acceptable access for waste collection vehicles (Cambridge Local Plan 2006 policy 3/12).

- (iii) Condition 26: No occupation shall take place until an agreement on liability for damage to the carriageways within the development has been reached between the applicants and Cambridge City Council.

Reason: to ensure satisfactory arrangements for the collection of waste and recycling (Cambridge Local Plan 2006 policy 3/12).

- (iv) Pages 68 and 82 (last paragraph) amend time period to complete S106 agreement to 26 June 2013.

Reasons for Approval

1. This development has been approved subject to conditions and the prior completion of a section 106 planning obligation (/a unilateral undertaking), because subject to those requirements it is considered to conform to the Development Plan as a whole, particularly the following policies:

Cambridgeshire and Peterborough Structure Plan 2003: 6/1, 9/8, 9/9

Cambridge Local Plan (2006): 3/1, 3/4, 3/6, 3/7, 3/8, 3/11, 3/12, 4/2, 4/4, 4/13, 4/15, 5/1, 5/5, 5/9, 5/10, 5/14, 6/1, 8/1, 8/2, 8/3, 8/4, 8/10, 8/16, 8/18, 10/1

2. The decision has been made having had regard to all other material planning considerations, including: the uplift in on-site open space provision compared to the previous refusal under application 11/0008/FUL; and the most appropriate form of mitigation for the loss of the existing sports pitch on the City FC site. The objection from Sport England, the FA and Cambridgeshire FA on the loss of the pitch and their recommendation for like-for-like replacement provision, as opposed to enhancement of sports facilities and access to pitches locally, was taken into account and considered alongside policy 4/2 of the Cambridge Local Plan, paragraph 74 of the NPPF 2012 and Sport England's own policy exception criteria E4. The Council considered the alternative sports and recreational provision put forward by the applicants in their latest S106 package and the local support for this. It was concluded that the recreational benefits to be gained in East and West Chesterton

Wards, in reflection of local need, outweighed the potential benefits of equivalent or better provision elsewhere. This was in light of the lack of other suitable locations for like-for-like provision that had a realistic chance of gaining planning permission at the time of considering the proposal. All other objections were taken into account, none of which were considered to have been of such significance as to justify doing other than grant planning permission.

3. In reaching this decision the local planning authority has acted on guidance provided by the National Planning Policy Framework, specifically paragraphs 186 and 187. The local planning authority has worked proactively with the applicant to bring forward a high quality development that will improve the economic, social and environmental conditions of the area.

These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.

Unless prior agreement has been obtained from the Head of Planning, in consultation with the Chair and Spokesperson of this Committee to extend the period for completion of the Planning Obligation required in connection with this development, if the Obligation has not been completed by 8 May 2013, it is recommended that the application be refused for the following reason(s):

The proposed development does not make appropriate provision for open space arising from the development needs of occupiers, appropriate provision for the loss of existing pitch, community development facilities, education and life-long learning facilities, transport mitigation measures, affordable housing, public art, waste storage, waste management facilities and monitoring in accordance with Cambridge Local Plan 2006 policies 3/7, 3/8, 3/12, 5/5, 5/14, 8/3 and 10/1, Cambridgeshire and Peterborough Structure Plan 2003 policies P6/1 and P9/8 and as detailed in the Planning Obligation Strategy 2010, the Affordable Housing Supplementary Planning Document 2008, the Public Art Supplementary Planning Document 2010, the RECAP Waste Management Design Guide SPD 2012, the Open Space Standards Guidance for Interpretation and Implementation 2010, the Northern Corridor Area Transport Plan 2003.

13/10/Planb 12/1444/S73 - Station Area Redevelopment

The Committee received an application for minor material amendments to outline planning permission.

The application sought approval for minor material amendments to outline planning permission reference 08/0266/OUT (the cb1 masterplan outline application) comprising an alteration to conditions 4 and 5 to enable adjustments to be made to the footprint of Block H1, to allow the expansion of retail space at ground floor in Block H1 and to allow the use of private open space to the east of Block H1 to include servicing and cycle storage

Mr Shepherd (Applicant's Architect) addressed the Committee in support of the application.

The Committee:

Resolved (unanimously) to accept the officer recommendation to approve planning permission as per the agenda.

Reasons for Approval

1. This development has been approved, conditionally, because subject to those requirements it is considered to generally conform to the Development Plan, particularly the following policies:

Cambridgeshire and Peterborough Structure Plan 2003: P6/1, P9/8 and P9/9.

Cambridge Local Plan 2006: 3/1, 3/2, 3/3, 3/4, 3/5, 3/6, 3/7, 3/8, 3/11, 3/12, 3/13, 3/15, 4/3, 4/4, 4/6, 4/7, 4/9, 4/10, 4/11, 4/12, 4/13, 4/14, 4/15, 4/16, 5/1, 5/5, 5/9, 5/10, 5/11, 5/12, 5/13, 5/14, 6/3, 6/8, 6/10, 7/1, 7/2, 7/7, 7/9, 7/10, 8/1, 8/2, 8/3, 8/4, 8/5, 8/6, 8/7, 8/9, 8/10, 8/11, 8/13, 8/16, 8/18, 9/1, 9/2, 9/9, 10/1.

2. The decision has been made having had regard to all other material planning considerations, none of which was considered to have been of such significance as to justify doing other than grant planning permission.

3. In reaching this decision the local planning authority has acted on guidance provided by the National Planning Policy Framework, specifically paragraphs 186 and 187. The local planning authority has worked proactively with the applicant to bring forward a high quality development that will improve the economic, social and environmental conditions of the area.

These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.

13/10/Planc 12/1445/REM - Station Area Redevelopment

The Committee received an application for approval of reserved matters.

The application sought approval for approval of reserved matters (appearance, layout, scale and landscape) for the second part of the phase 1B of the CB1 Masterplan (ref:08/0266/OUT), comprising H1 for 354 units of student accommodation and 351 sqm of retail (A1/A3/A4/A5) with associated cycle parking, car parking (disabled and warden only) and landscaping.

Mr Shepherd (Applicant's Architect) addressed the Committee in support of the application.

Councillor Saunders proposed an amendment to the Officer's recommendation that Committee members to be invited to view sample panel.

This amendment was **carried unanimously**.

The Committee:

Resolved (unanimously) to accept the officer recommendation to approve planning permission as per the agenda with additional condition 16 set out below. Committee members to be invited to view sample panel (condition 2) but content that this condition be discharged under delegated powers.

(Amendment to last informative to refer to drawing no. 02-03-013 and 02-05-005-C in the light of amended plan)

Condition 16: Management of Cycle Parking

Prior to the commencement of development, with the exception of below ground works, a Cycle Parking Management Plan shall be submitted to and approved by the local planning authority in writing. The approved Cycle Parking Management Plan shall thereafter be first implemented upon first occupation of the development and shall be maintained and implemented unless otherwise agreed in writing by the local planning authority.

Reason: To ensure satisfactory arrangements are in place to secure the management of cycle parking.(Cambridge Local Plan 2006 policy 8/6).

Reasons for Approval

1. This development has been approved, conditionally, because subject to those requirements it is considered to generally conform to the Development Plan, particularly the following policies:

Cambridge Local Plan (2006): 3/1, 3/3, 3/4, 3/7, 3/8, 3/11, 3/12, 3/13, 3/15, 4/10, 4/11, 4/12, 4/13, 4/14, 4/15, 6/8, 6/10, 7/10, 8/1, 8/2, 8/4, 8/6, 8/8, 8/9, 8/10, 8/16, 8/17, 8/18, 9/1, 9/2, 9/9 and 10/1.

2. The decision has been made having had regard to all other material planning considerations, none of which was considered to have been of such significance as to justify doing other than grant planning permission.
3. In reaching this decision the local planning authority has acted on guidance provided by the National Planning Policy Framework, specifically paragraphs 186 and 187. The local planning authority has worked proactively with the applicant to bring forward a high quality development that will improve the economic, social and environmental conditions of the area.

These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.

13/10/Pland 12/1496/FUL - Nuffield Hospital, 4 Trumpington Road

The Committee received an application for full planning permission.

The application sought approval for erection of new hospital with associated external works (includes demolition of existing buildings).

Mr Hayes (Applicant's Representative) addressed the Committee in support of the application.

The City Development Manager proposed an amendment to the Officer's recommendation to discount the following amendment to text:

10.0 – Recommendation:

Sub-paragraph should read:

1. APPROVE subject to the satisfactory completion of the s106 agreement within 3 months from the date of this permission and subject to the following conditions and reasons for approval:

This amendment was **carried unanimously**.

The Committee:

Resolved (by 5 votes to 0) to accept the officer recommendation to approve planning permission as per the agenda, subject to conditions 1 to 26. Wording relating to S106 in the amendment sheet (above) would be revoked and condition 26 (public art provision) would be reworded. The draft wording of condition 26 would be circulated to chairs and spokes for agreement before being formally attached to the decision notice.

Reasons for Approval

1. This development has been approved subject to conditions and the prior completion of a section 106 planning obligation (a unilateral undertaking), because subject to those requirements it is considered to conform to the Development Plan as a whole, particularly the following policies:

Cambridge Local Plan (2006): 3/4, 3/7, 3/11, 3/12 and 5/12.

2. The decision has been made having had regard to all other material planning considerations, none of which was considered to have been of such significance as to justify doing other than grant planning permission.
3. In reaching this decision the local planning authority has acted on guidance provided by the National Planning Policy Framework, specifically paragraphs 186 and 187. The local planning authority has worked proactively with the applicant to bring forward a high quality development that will improve the economic, social and environmental conditions of the area.

These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.

13/10/Plane 12/1512/FUL - 1 Benson Place

The report was withdrawn from the agenda so Officers could seek further information and would come to March instead.

13/10/Planf 12/1513/CAC - 1 Benson Place

The report was withdrawn from the agenda so Officers could seek further information and would come to March instead.

Re-Ordering Agenda

Under paragraph 4.2.1 of the Council Procedure Rules, the Chair used her discretion to alter the order of the agenda items. However, for ease of the reader, these minutes will follow the order of the agenda.

13/11/Plan General Items

13/11/Plana 12/1321/FUL Motor Ace Car Sales, 87 East Road

The Committee received a request to agree the conditions for planning application ref 12/1321/FUL as Planning Committee resolved to grant planning permission for the development on 9 January 2013.

The City Development Manager, Sarah Dyer, pointed out that the purpose of the report was to seek agreement on the proposed conditions, not on the reasons for approving the application, as the latter were agreed at Planning Committee on 9 January 2013.

The planning case officer, Mr Sav Patel, briefly explained the number and type of conditions recommended for members' approval.

The report sought approval for draft conditions, as set out in the Officer's report, so that the planning application can be formally determined subject to the completion of a S106 agreement.

The Committee:

Resolved (unanimously) to accept the officer recommendation to approve the draft conditions to be attached to the planning permission.

Exclusion of the Press and Public

The Planning Committee resolved to exclude members of the public from the meeting on the grounds that, if they were present, there would be disclosure to them of information defined as exempt from publication by virtue of paragraph 5 of Part 1 of Schedule 12A of the Local Government Act 1972.

13/11/Planb 11/1534/FUL St Colette's Preparatory School

Planning application ref. 11/1534/FUL was refused at Planning Committee on 4 April 2012. An appeal has been lodged. Planning application ref. 12/1040/FUL (the revised application) has not been determined but was presented to Planning Committee on 14 November 2012.

The Committee were asked to note the change in procedure for dealing with the Appeal and confirm reasons for refusal.

The Committee:

Resolved (unanimously) to accept the officer recommendation to:

- (i) Note the change in procedure for dealing with the Appeal against the decision to refuse application reference 11/1534/FUL.
- (ii) Confirm that the determination of application ref. 12/1040/FUL be deferred until the Inspector's decision regarding the Appeal of application 11/1534/FUL is known and can be reported back to the Planning Committee.

Resolved (5 votes to 1) to accept the officer recommendation to:

- (iii) Confirm that the Council's case will relate to Reasons for Refusal 1, 3, 4 and 5 only in relation to the Appeal against the decision to refuse application reference 11/1534/FUL.

13/12/Plan Tree Items

13/12/Plana Tree Works Application No. 12/531/TTCA - Pinehurst South, Grange Road

The Committee received an application to remove a young Blue Atlas Cedar from the grounds of Pinehurst South, Grange Road.

The Committee:

Resolved (unanimously) to accept the officer recommendation and offer no formal objection to the tree's removal.

13/12/Planb Tree Works Application No. 12/436/TTPO - Church Street

The Committee received an application to reduce the length of selected branches and crown lift the two Horse Chestnut trees at the corner of Church Street and Lynfield Road.

The Committee:

Resolved (unanimously) to accept the officer recommendation and grant consent subject to condition.

The meeting ended at 1.00 pm

CHAIR

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APPENDIX 1 – DEVELOPMENT PLAN POLICY, PLANNING GUIDANCE AND MATERIAL CONSIDERATIONS

1.0 Central Government Advice

- 1.1 National Planning Policy Framework (March 2012)** – sets out the Government’s economic, environmental and social planning policies for England. These policies articulate the Government’s vision of sustainable development, which should be interpreted and applied locally to meet local aspirations.
- 1.2 Circular 11/95 – The Use of Conditions in Planning Permissions:** Advises that conditions should be necessary, relevant to planning, relevant to the development permitted, enforceable, precise and reasonable in all other respects.
- 1.3 Community Infrastructure Levy Regulations 2010** – places a statutory requirement on the local authority that where planning permission is dependent upon a planning obligation the obligation must pass the following tests:
 - (a) necessary to make the development acceptable in planning terms;
 - (b) directly related to the development; and
 - (c) fairly and reasonably related in scale and kind to the development.

2.0 Cambridgeshire and Peterborough Structure Plan 2003

Planning Obligation Related Policies

P6/1 Development-related Provision

P9/8 Infrastructure Provision

P9/9 Cambridge Sub-Region Transport Strategy

3.0 Cambridge Local Plan 2006

3/1 Sustainable development

3/3 Setting of the City

3/4 Responding to context

3/6 Ensuring coordinated development

3/7 Creating successful places

3/9 Watercourses and other bodies of water

3/10 Subdivision of existing plots

3/11 The design of external spaces

3/12 The design of new buildings

3/13 Tall buildings and the skyline

3/14 Extending buildings

3/15 Shopfronts and signage

- 4/1 Green Belt
- 4/2 Protection of open space
- 4/3 Safeguarding features of amenity or nature conservation value
- 4/4 Trees
- 4/6 Protection of sites of local nature conservation importance
- 4/8 Local Biodiversity Action Plans
- 4/9 Scheduled Ancient Monuments/Archaeological Areas
- 4/10 Listed Buildings
- 4/11 Conservation Areas
- 4/12 Buildings of Local Interest
- 4/13 Pollution and amenity
- 4/14 Air Quality Management Areas
- 4/15 Lighting

- 5/1 Housing provision
- 5/2 Conversion of large properties
- 5/3 Housing lost to other uses
- 5/4 Loss of housing
- 5/5 Meeting housing needs
- 5/7 Supported housing/Housing in multiple occupation
- 5/8 Travellers
- 5/9 Housing for people with disabilities
- 5/10 Dwelling mix
- 5/11 Protection of community facilities
- 5/12 New community facilities
- 5/15 Addenbrookes

- 6/1 Protection of leisure facilities
- 6/2 New leisure facilities
- 6/3 Tourist accommodation
- 6/4 Visitor attractions
- 6/6 Change of use in the City Centre
- 6/7 Shopping development and change of use in the District and Local Centres
- 6/8 Convenience shopping
- 6/9 Retail warehouses
- 6/10 Food and drink outlets.

- 7/1 Employment provision
- 7/2 Selective management of the Economy
- 7/3 Protection of Industrial and Storage Space
- 7/4 Promotion of cluster development
- 7/5 Faculty development in the Central Area, University of Cambridge
- 7/6 West Cambridge, South of Madingley Road
- 7/7 College and University of Cambridge Staff and Student Housing
- 7/8 Anglia Ruskin University East Road Campus
- 7/9 Student hostels for Anglia Ruskin University
- 7/10 Speculative Student Hostel Accommodation
- 7/11 Language Schools

- 8/1 Spatial location of development
- 8/2 Transport impact
- 8/4 Walking and Cycling accessibility
- 8/6 Cycle parking
- 8/8 Land for Public Transport
- 8/9 Commercial vehicles and servicing
- 8/10 Off-street car parking
- 8/11 New roads
- 8/12 Cambridge Airport
- 8/13 Cambridge Airport Safety Zone
- 8/14 Telecommunications development
- 8/15 Mullard Radio Astronomy Observatory, Lords Bridge
- 8/16 Renewable energy in major new developments
- 8/17 Renewable energy
- 8/18 Water, sewerage and drainage infrastructure

- 9/1 Further policy guidance for the Development of Areas of Major Change
- 9/2 Phasing of Areas of Major Change
- 9/3 Development in Urban Extensions
- 9/5 Southern Fringe
- 9/6 Northern Fringe
- 9/7 Land between Madingley Road and Huntingdon Road
- 9/8 Land between Huntingdon Road and Histon Road
- 9/9 Station Area

- 10/1 Infrastructure improvements

Planning Obligation Related Policies

- 3/7 Creating successful places
- 3/8 Open space and recreation provision through new development
- 3/12 The Design of New Buildings (*waste and recycling*)
- 4/2 Protection of open space
- 5/13 Community facilities in Areas of Major Change
- 5/14 Provision of community facilities through new development
- 6/2 New leisure facilities
- 8/3 Mitigating measures (*transport*)
- 8/5 Pedestrian and cycle network
- 8/7 Public transport accessibility
- 9/2 Phasing of Areas of Major Change
- 9/3 Development in Urban Extensions
- 9/5 Southern Fringe
- 9/6 Northern Fringe
- 9/8 Land between Huntingdon Road and Histon Road
- 9/9 Station Area
- 10/1 Infrastructure improvements (*transport, public open space, recreational and community facilities, waste recycling, public realm, public art, environmental aspects*)

4.0 **Supplementary Planning Documents**

- 4.1 **Cambridge City Council (May 2007) – Sustainable Design and Construction:** Sets out essential and recommended design considerations of relevance to sustainable design and construction. Applicants for major developments are required to submit a sustainability checklist along with a corresponding sustainability statement that should set out information indicated in the checklist. Essential design considerations relate directly to specific policies in the Cambridge Local Plan 2006. Recommended considerations are ones that the council would like to see in major developments. Essential design considerations are urban design, transport, movement and accessibility, sustainable drainage (urban extensions), energy, recycling and waste facilities, biodiversity and pollution. Recommended design considerations are climate change adaptation, water, materials and construction waste and historic environment.
- 4.2 **Cambridgeshire and Peterborough Waste Partnership (RECAP): Waste Management Design Guide Supplementary Planning Document (February 2012):** The Design Guide provides advice on the requirements for internal and external waste storage, collection and recycling in new residential and commercial developments. It provides advice on assessing planning applications and developer contributions.
- 4.3 **Cambridge City Council (January 2008) - Affordable Housing:** Gives advice on what is involved in providing affordable housing in Cambridge. Its objectives are to facilitate the delivery of affordable housing to meet housing needs and to assist the creation and maintenance of sustainable, inclusive and mixed communities.
- 4.4 **Cambridge City Council (March 2010) – Planning Obligation Strategy:** provides a framework for securing the provision of new and/or improvements to existing infrastructure generated by the demands of new development. It also seeks to mitigate the adverse impacts of development and addresses the needs identified to accommodate the projected growth of Cambridge. The SPD addresses issues including transport, open space and recreation, education and life-long learning, community facilities, waste and other potential development-specific requirements.
- 4.5 **Cambridge City Council (January 2010) - Public Art:** This SPD aims to guide the City Council in creating and providing public art in Cambridge by setting out clear objectives on public art, a clarification of policies, and the means of implementation. It covers public art delivered through the planning process, principally Section 106 Agreements (S106), the commissioning of public art using the S106 Public Art Initiative, and outlines public art policy guidance.

4.6 **Old Press/Mill Lane Supplementary Planning Document (January 2010)** Guidance on the redevelopment of the Old Press/Mill Lane site.

Eastern Gate Supplementary Planning Document (October 2011)

Guidance on the redevelopment of the Eastern Gate site. The purpose of this development framework (SPD) is threefold:

- To articulate a clear vision about the future of the Eastern Gate area;
- To establish a development framework to co-ordinate redevelopment within
- the area and guide decisions (by the Council and others); and
- To identify a series of key projects, to attract and guide investment (by the Council and others) within the area.

5.0 **Material Considerations**

Central Government Guidance

5.1 **Letter from Secretary of State for Communities and Local Government (27 May 2010)**

The coalition government is committed to rapidly abolish Regional Strategies and return decision making powers on housing and planning to local councils. Decisions on housing supply (including the provision of travellers sites) will rest with Local Planning Authorities without the framework of regional numbers and plans.

5.2 **Written Ministerial Statement: Planning for Growth (23 March 2011)**

Includes the following statement:

When deciding whether to grant planning permission, local planning authorities should support enterprise and facilitate housing, economic and other forms of sustainable development. Where relevant and consistent with their statutory obligations they should therefore:

(i) consider fully the importance of national planning policies aimed at fostering economic growth and employment, given the need to ensure a return to robust growth after the recent recession;

(ii) take into account the need to maintain a flexible and responsive supply of land for key sectors, including housing;

(iii) consider the range of likely economic, environmental and social benefits of proposals; including long term or indirect benefits such as increased consumer choice, more viable communities and more robust local economies (which may, where relevant, include matters such as job creation and business productivity);

(iv) be sensitive to the fact that local economies are subject to change and so take a positive approach to development where new economic data suggest that prior assessments of needs are no longer up-to-date;

(v) ensure that they do not impose unnecessary burdens on development.

In determining planning applications, local planning authorities are obliged to have regard to all relevant considerations. They should ensure that they give appropriate weight to the need to support economic recovery, that applications that secure sustainable growth are treated favourably (consistent with policy in PPS4), and that they can give clear reasons for their decisions.

5.3 City Wide Guidance

Arboricultural Strategy (2004) - City-wide arboricultural strategy.

Biodiversity Checklist for Land Use Planners in Cambridgeshire and Peterborough (March 2001) - This document aims to aid strategic and development control planners when considering biodiversity in both policy development and dealing with planning proposals.

Cambridge Landscape and Character Assessment (2003) – An analysis of the landscape and character of Cambridge.

Cambridge City Nature Conservation Strategy (2006) – Guidance on habitats should be conserved and enhanced, how this should be carried out and how this relates to Biodiversity Action Plans.

Criteria for the Designation of Wildlife Sites (2005) – Sets out the criteria for the designation of Wildlife Sites.

Cambridge City Wildlife Sites Register (2005) – Details of the City and County Wildlife Sites.

Cambridge and South Cambridgeshire Strategic Flood Risk Assessment (November 2010) - a tool for planning authorities to identify and evaluate the extent and nature of flood risk in their area and its implications for land use planning.

Strategic Flood Risk Assessment (2005) – Study assessing the risk of flooding in Cambridge.

Cambridge and Milton Surface Water Management Plan (2011) – A SWMP outlines the preferred long term strategy for the management of surface water. Alongside the SFRA they are the starting point for local flood risk management.

Cambridge City Council (2011) - Open Space and Recreation

Strategy: Gives guidance on the provision of open space and recreation facilities through development. It sets out to ensure that open space in Cambridge meets the needs of all who live, work, study in or visit the city and provides a satisfactory environment for nature and enhances the local townscape, complementing the built environment.

The strategy:

- sets out the protection of existing open spaces;
- promotes the improvement of and creation of new facilities on existing open spaces;
- sets out the standards for open space and sports provision in and through new development;
- supports the implementation of Section 106 monies and future Community Infrastructure Levy monies

As this strategy suggests new standards, the Cambridge Local Plan 2006 standards will stand as the adopted standards for the time-being. However, the strategy's new standards will form part of the evidence base for the review of the Local Plan

Balanced and Mixed Communities – A Good Practice Guide (2006)

– Produced by Cambridgeshire Horizons to assist the implementation of the Areas of Major Change.

Green Infrastructure Strategy for the Cambridgeshire Sub-Region (2006)

- Produced by Cambridgeshire Horizons to assist the implementation of the Areas of Major Change and as a material consideration in the determination of planning applications and appeals.

A Major Sports Facilities Strategy for the Cambridge Sub-Region (2006)

- Produced by Cambridgeshire Horizons to assist the implementation of the Areas of Major Change.

Cambridge Sub-Region Culture and Arts Strategy (2006)

Produced by Cambridgeshire Horizons to assist the implementation of the Areas of Major Change.

Cambridgeshire Quality Charter for Growth (2008) – Sets out the core principles of the level of quality to be expected in new developments in the Cambridge Sub-Region

Cambridge City Council - Guidance for the application of Policy 3/13 (Tall Buildings and the Skyline) of the Cambridge Local Plan (2006) (2012) - sets out in more detail how existing council policy can

be applied to proposals for tall buildings or those of significant massing in the city.

Cambridge Walking and Cycling Strategy (2002) – A walking and cycling strategy for Cambridge.

Protection and Funding of Routes for the Future Expansion of the City Cycle Network (2004) – Guidance on how development can help achieve the implementation of the cycle network.

Cambridgeshire Design Guide For Streets and Public Realm (2007): The purpose of the Design Guide is to set out the key principles and aspirations that should underpin the detailed discussions about the design of streets and public spaces that will be taking place on a site-by-site basis.

Cycle Parking Guide for New Residential Developments (2010) – Gives guidance on the nature and layout of cycle parking, and other security measures, to be provided as a consequence of new residential development.

Air Quality in Cambridge – Developers Guide (2008) - Provides information on the way in which air quality and air pollution issues will be dealt with through the development control system in Cambridge City. It compliments the Sustainable Design and Construction Supplementary Planning Document.

The Cambridge Shopfront Design Guide (1997) – Guidance on new shopfronts.

Roof Extensions Design Guide (2003) – Guidance on roof extensions.

Modelling the Costs of Affordable Housing (2006) – Toolkit to enable negotiations on affordable housing provision through planning proposals.

5.6 Area Guidelines

Cambridge City Council (2003)–Northern Corridor Area Transport Plan:

Cambridge City Council (2002)–Southern Corridor Area Transport Plan:

Cambridge City Council (2002)–Eastern Corridor Area Transport Plan:

Cambridge City Council (2003)–Western Corridor Area Transport Plan:

The purpose of the Plan is to identify new transport infrastructure and service provision that is needed to facilitate large-scale development and to identify a fair and robust means of calculating how individual

development sites in the area should contribute towards a fulfilment of that transport infrastructure.

Buildings of Local Interest (2005) – A schedule of buildings of local interest and associated guidance.

Brooklands Avenue Conservation Area Appraisal (2002)
Cambridge Historic Core Conservation Area Appraisal (2006)
Storeys Way Conservation Area Appraisal (2008)
Chesterton and Ferry Lane Conservation Area Appraisal (2009)
Conduit Head Road Conservation Area Appraisal (2009)
De Freville Conservation Area Appraisal (2009)
Kite Area Conservation Area Appraisal (1996)
Newnham Croft Conservation Area Appraisal (1999)
Southacre Conservation Area Appraisal (2000)
Trumpington Conservation Area Appraisal (2010)
Mill Road Area Conservation Area Appraisal (2011)
West Cambridge Conservation Area Appraisal (2011)

Guidance relating to development and the Conservation Area including a review of the boundaries.

Jesus Green Conservation Plan (1998)
Parkers Piece Conservation Plan (2001)
Sheeps Green/Coe Fen Conservation Plan (2001)
Christs Pieces/New Square Conservation Plan (2001)

Historic open space guidance.

Hills Road Suburbs and Approaches Study (March 2012)
Long Road Suburbs and Approaches Study (March 2012)
Barton Road Suburbs and Approaches Study (March 2009)
Huntingdon Road Suburbs and Approaches Study (March 2009)
Madingley Road Suburbs and Approaches Study (March 2009)
Newmarket Road Suburbs and Approaches Study (October 2011)

Provide assessments of local distinctiveness which can be used as a basis when considering planning proposals

Station Area Development Framework (2004) – Sets out a vision and Planning Framework for the development of a high density mixed use area including new transport interchange and includes the **Station Area Conservation Appraisal**.

Southern Fringe Area Development Framework (2006) – Guidance which will help to direct the future planning of development in the Southern Fringe.

West Cambridge Masterplan Design Guidelines and Legal Agreement (1999) – Sets out how the West Cambridge site should be developed.

Mitcham's Corner Area Strategic Planning and Development Brief (2003) – Guidance on the development and improvement of Mitcham's Corner.

Mill Road Development Brief (Robert Sayle Warehouse and Co-Op site) (2007) – Development Brief for Proposals Site 7.12 in the Cambridge Local Plan (2006)

Agenda Item 4a

PLANNING COMMITTEE

Date: 6th March 2013

Application Number	12/1556/FUL	Agenda Item	
Date Received	10th December 2012	Officer	Mrs Sarah Dyer
Target Date	11th March 2013		
Ward	Trumpington		
Site	32 - 38 Station Road Cambridge Cambridgeshire CB1 2JH		
Proposal	The demolition of 32-38 Station Road and the construction of two new office buildings comprising 7279 sqm of office floorspace (class B1) for 50 Station Road and 8621 sqm of office floorspace (class B1) and 271 sqm of retail/cafe space (class A1/A3) for 60 Station Road as a phased development, including ancillary accommodation/facilities with an additional single level basement to both buildings and up to 76 car parking spaces, with associated plant, up to 576 internal and external cycle parking spaces, realignment of the northern part of the southern access road, and hard and soft landscaping.		
Applicant	Brookgate CB1 Ltd C/o Agent		

SUMMARY	<p>The development accords with the Development Plan for the following reasons:</p> <p>The proposed building is of a scale, massing and design which are appropriate to its setting within an Area of Major Change and of a sufficiently high quality to justify the removal of Buildings of Local Interest.</p> <p>The Outline consent for the Station Area development is a very significant material consideration and the development accords with that consent in all regards with the exception of site area.</p>
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	<p>The application includes mitigation measures to ensure that all of the impacts of the development are dealt with both independently and as part of the wider Masterplan</p> <p>This revised application addresses the reasons for refusal that were agreed by Planning Committee in relation to application reference 12/0502/FUL</p>
RECOMMENDATION	APPROVAL

0.0 INTRODUCTION

- 0.1 This application and the associated application for Conservation Area Consent (CAC) for the demolition of Wilton Terrace/32-38 Station Road follow on from a refusal of planning permission and CAC by the Planning Committee in July 2012. This decision was taken against officer recommendation and is the subject of an appeal to the Planning Inspectorate which is to be held by Public Inquiry in May/June this year.
- 0.2 The reasons for refusal of the earlier planning application (ref. 12/0502/FUL) were as follows:
1. The proposed building by virtue of its overall scale and massing would have an overly dominant impact on the Station Road frontage to the detriment of the streetscene and the Conservation Area contrary to policies 3/4, 3/7, 3/12 and 4/11 of the Cambridge local Plan 2006.
 2. The development fails to make adequate provision for car parking which would be likely to result in overspill parking into nearby residential areas, which would have a detrimental impact on the amenity of residents of those areas. The development is therefore contrary to policy 8/10 of the Cambridge Local Plan 2006.
 3. The public benefit arising from the development fails to provide sufficient justification for the demolition of Buildings of Local Interest, which are recognised as heritage assets. The development is therefore contrary to policy 4/12 of the

Cambridge Local Plan 2006 and to guidance provided by the National Planning Policy Framework.

4. The proposed development does not make appropriate provision for transport mitigation measures/infrastructure provision, mitigation of potential for overspill parking, the funding and agreement of the Travel Plan Co-ordinator, public art, relocation of a community facility, restriction on occupation of offices and monitoring in accordance with Cambridge Local Plan 2006 policies 3/7, 5/11, 7/2, 8/3, 9/9 and 10/1. Cambridgeshire and Peterborough Structure Plan 2003 policies P6/1, P9/8 and P9/9 and as detailed in the Planning Obligation Strategy 2010, the Public Art Supplementary Planning Document 2010 and the Southern Corridor Area Transport Plan 2002.

0.3 The associated Conservation Area consent (CAC) application (Ref. 12/0496/CAC) was also refused for the following reasons:

1. The proposed demolition is contrary to policies 4/11 and 4/12 of the Cambridge Local Plan (2006) and paragraph 136 of the National Planning Policy Framework 2012 in that in the absence of an approved redevelopment scheme that has a contract for redevelopment and which preserves and enhances the character or appearance of the Conservation Area by faithfully reflecting its context or providing a contrast with it, the demolition of the buildings would result in the loss of a heritage asset in the form of Buildings of Local Interest which contribute positively to the character and appearance of the Conservation Area.
2. The public benefit from the development fails to provide sufficient justification for the demolition of Buildings of Local Interest, which are recognised as heritage assets. The development is therefore contrary to policy 4/12 of the Cambridge Local Plan 2006 and to guidance provided by the National Planning Policy Framework.

0.4 These previous decisions are a very significant material consideration in the assessment and determination of the current applications. For this reason my Report focusses on the key differences between the two schemes and whether the revised proposals overcome the reasons for refusal that are set

out above. It would be unwise to reject the current applications on grounds that were regarded as acceptable to the Council in relation to the first application unless there are demonstrable changes in circumstances.

- 0.5 The current application concerns a revised scheme which in the view of the applicants overcomes the earlier reasons for refusal. The key areas of difference between the refused scheme and the current scheme are in terms of the design of the building, the amount of car parking and a fuller explanation of the public benefits that justify the demolition of 32-38 Wilton Terrace, which are Buildings of Local Interest (BLI).
- 0.6 The mitigation measures and controls that can be delivered through the planning obligations remain unchanged and if this Agreement is completed it will address the fourth reason for refusal of the earlier scheme.
- 0.7 With the exception of the revocation of the East of England Plan the planning policy context against which a decision must be made is unchanged in comparison with that which applied in July 2012. The following report describes and assesses the current scheme and where appropriate makes comparisons with the earlier scheme.

1.0 SITE DESCRIPTION/AREA CONTEXT

- 1.1 The application site forms part of a larger area, which is the subject of the CB1 Station Area Redevelopment proposals for which outline planning permission was granted in April 2010. The site lies on the south side of Station Road and to the east of the access serving the Warren Close development. The western half of the site is occupied by 32-38 Station Road (Wilton Terrace) which accommodates Woodlands Doctors Surgery and Brookgate's Offices (the applicants). The eastern half of the site was previously occupied by offices serving the Rank Hovis site that have been demolished as part of the CB1 development. This part of the site is currently in use as a contractor's compound.
- 1.2 To the south of the site is the Warren Close housing development and land which is under development for flats with commercial space at ground level. A six storey block of flats at Warren Close sits behind the western half of the site and

following the completion of development a public square and a seven storey block of flats will sit behind the eastern half. To the west the site is bounded by the access road serving Warren Close beyond which are office buildings. To the east is Murdoch House a three storey office block with undercroft which fronts the Station. To the north the site is bounded by Station Road beyond which is former Red House site that has planning permission for a hotel and the current station cycle park.

- 1.3 The site is within the Station Area Redevelopment Framework Boundary and within the Central Conservation Area No.1. 32-38 Station Road are buildings of Local Interest as are the Mill and Silo that sit to the southeast. The Station is a listed building. The site falls within the controlled parking zone.

2.0 THE PROPOSAL

- 2.1 Full planning permission is sought for an office development comprising two linked office blocks that will be known as 50 and 60 Station Road. 50 Station Road is to occupy the western part of the site and 60 the eastern part. The application differs from previous applications that have been brought forward within the CB1 Station Area site because it is not an application for reserved matters. In this case a reserved matters application could not be submitted because the layout of the block does not conform to the approved Parameter Plans. The application also includes a realignment of the Southern Access Road (SAR). A separate application for Conservation Area Consent for the demolition of 32-38 Station Road has also been made. A report to address this application appears elsewhere on the Agenda. A Non Material Amendment for the realignment of the SAR was approved in July 2012.
- 2.2 Although the application is not constrained by the Outline permission, the approved Parameter Plans which represent the approved Masterplan are a significant material consideration in the assessment of the application. I will make reference to the Masterplan throughout my Assessment as the applicants have done in the supporting material for the application.
- 2.3 In total the two office buildings will deliver 15,900 sq. m of office floor space compared with 16427 sq. m in the earlier scheme; 7279 sq. m in 50 Station Road (compared with 7806 sq. m) and

8621 sq. m in 60 Station Road 9 (identical to earlier scheme). In addition to this floorspace 271 sq. m of retail space is provided in the south east corner of 60 Station Road where it fronts the public square and in a 'pod' which between the two blocks on the Station Road frontage. Car and cycle parking is provided within the blocks with further cycle parking within the landscaped areas around the buildings. The SAR is realigned further to the east. Further details of the proposals are set out in my Assessment

2.4 As submitted, the key differences between this scheme and the previous scheme were that the facade of no 50 was set back from its previous alignment by 3 metre and the line of the main parapet to no. 50 was dropped to the level of the roof terrace balustrade. Fifteen additional car parking spaces were proposed in the basement. Following feedback from officers the parapet to no. 50 has been returned to its previous height.

2.5 The application is accompanied by the following supporting information:

1. Design and Access Statement by Grimshaw
2. Heritage Statement by Beacon Planning
3. Planning Statement by Savills
4. Acoustic Report by Hilson Moran
5. Air Quality Report by Hilson Moran
6. Archaeological Statement by Cambridge Archaeological Unit
7. BREEAM Pre-assessment report by Hilson Moran
8. Daylight Report by Mott Macdonald
9. Draft s106 Agreement by Mills and Reeve
10. Ecology Report by RPS
11. Energy and Sustainability Statement by Hilson Moran
12. Estate Management Strategy by Bidwells
13. Surface/foul water strategy by Mott Macdonald
14. Ground Contamination report by Mott Macdonald
15. Landscape Proposals by Robert Myers
16. Landscape Management Plan by Robert Myers
17. Sustainability Checklist by Hilson Moran
18. Transport Assessment (inc. Travel Plan) by Mott Macdonald
19. Waste Management Strategy by Mott Macdonald

2.6 The application has been amended in the following ways:

- ☐ Revised Ecology Report in response to comments by Nature Conservation Officer
- ☐ Amended plans in response to comments by made by the UDC team specifically stone columns extended on the North and South frontages of no. 50, clarification of details on first floor plan and rearrangement of office floorspace/cycle parking in no. 50.

3.0 SITE HISTORY

Reference	Description	Outcome
05/1166	Conservation Area Consent for demolition buildings on the Rank Hovis site	A/C
06/0266/OUT	CB1 Station Area Redevelopment	A/C
09/0031	Conservation Area Consent for demolition buildings on the Rank Hovis site	A/C
11/1303/FUL	Demolition of 32 – 38 Station Road and erection of two office buildings	Withdrawn
11/1351/CAC	Conservation Area Consent for demolition of 32-38 Station Road	Withdrawn
12/0502/FUL	Demolition of 32 – 38 Station Road and erection of two office buildings	Refused
12/0496/CAC	Conservation Area Consent for demolition of 32-38 Station Road	Refused
	Non Material Amendment for Realignment of SAR	Approved

12/1553/CAC	Conservation Area Consent for demolition of 32-38 Station Road	Pending consideratio n
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4.0 **PUBLICITY**

4.1	Advertisement:	Yes
	Adjoining Owners:	Yes
	Site Notice Displayed:	Yes
	Public Meeting/Exhibition	No
	DC Forum	No

5.0 **POLICY**

5.1 See Appendix 1 for full details of Central Government Guidance, Cambridgeshire and Peterborough Structure Plan 2003 policies, Cambridge Local Plan 2006 policies, Supplementary Planning Documents and Material Considerations.

5.2 Relevant Development Plan policies

PLAN	POLICY NUMBER
Cambridgeshire and Peterborough Structure Plan 2003	P6/1 P9/8 P9/9
Cambridge Local Plan 2006	3/1 3/4 3/6 3/7 3/11 3/12 3/13 4/4 4/11 4/12 4/13 4/14 4/15 5/11 7/2 8/2 8/4 8/6 8/9 8/10 8/16 8/18 9/1 9/9 10/1

5.3 Relevant Central Government Guidance, Supplementary Planning Documents and Material Considerations

Central Government Guidance	<p>National Planning Policy Framework March 2012</p> <p>Circular 11/95</p> <p>Community Infrastructure Levy Regulations 2010</p>
Supplementary Planning Documents	<p>Sustainable Design and Construction</p> <p>Waste Management Design Guide</p> <p>Planning Obligation Strategy</p> <p>Public Art</p>
Material Considerations	<p><u>Central Government:</u></p> <p>Letter from Secretary of State for Communities and Local Government (27 May 2010)</p> <p>Written Ministerial Statement: Planning for Growth (23 March 2011)</p>
	<p><u>Citywide:</u></p> <p>Biodiversity Checklist</p> <p>Cambridge City Nature Conservation Strategy</p> <p>Cambridge and South Cambridgeshire Strategic Flood Risk Assessment</p> <p>Strategic Flood Risk Assessment (2005)</p> <p>Cambridge and Milton Surface Water Management Plan</p> <p>Cambridgeshire Quality Charter for Growth</p> <p>Cambridge City Council - Guidance for the application of Policy 3/13 (Tall Buildings and the Skyline) of the Cambridge Local Plan</p>

	(2006) (2012) Cambridge Walking and Cycling Strategy Cambridgeshire Design Guide For Streets and Public Realm Air Quality in Cambridge – Developers Guide Cambridge Shopfront Design Guide
	<u>Area Guidelines:</u> Southern Corridor Area Transport Plan Buildings of Local Interest Station Area Development Framework/Station Area Conservation Appraisal

6.0 CONSULTATIONS

Cambridgeshire County Council (Engineering)

- 6.1 The development will be a significant contributor to the need for improvement to the layout of the signal-controlled junction of Station Road with Hills Road which provides full pedestrian facilities at that junction.

The manoeuvring diagrams provided indicate that large cars could access the parking bays, although with some degree of constraint.

Detailed, dimensioned layouts of the cycle parking and the number of spaces located in each area are required.

Parking surveys of the residential area to the east of the railway are required in common with the Outline approval.

Conditions/informatives are recommended to address the need for a traffic management plan and works to the public highway.

Cambridgeshire County Council (New Communities)

6.2 *Car and Cycle Parking*

The TA identifies that the proposed office buildings will have limited on-site car parking, with only 76 spaces. This is based on a ratio of 1 space per 150 sqm instead of the maximum 1 space per 125 sqm that was agreed for B1 developments within the CB1 development area. Given the proximity of the site to the railway station, the bus services stopping at the railway station and good walking and cycling links to the city centre and to surrounding residential areas, it is considered acceptable to reduce on-site parking provision below the standard level.

However, there will be a requirement for on-street parking surveys to be undertaken prior to the commencement of the development (to obtain a baseline position without parking levels being skewed by construction traffic) and at an agreed time after completion and full occupation of the development. These surveys will need to cover the surrounding area where overspill parking from the development might be expected, and the timing and format of the surveys will need to be agreed prior to their commissioning.

The draft Section 106 Agreement (Nov 2012) submitted with the planning application makes a commitment for such surveys to take place, with the baseline survey proposed for two months after commencement, and the second survey one month after notification of the need for the survey is given by the County Council. The first of these trigger points should be amended so that the baseline survey is carried out prior to commencement of the development, to avoid any impact from construction traffic as noted above.

The draft Section 106 Agreement also identifies the sum of £75,000 to cover the County Council's costs associated with the implementation of a residents parking scheme if the County Council deems this necessary after analysing the results of the parking surveys. Subject to detailed wording of the Section 106 Agreement, this approach is reasonable.

The TA identifies that 576 cycle parking spaces will be provided at the development, split between a secure cycle pavilion located at the rear of the buildings, secure covered parking located between the two buildings, and uncovered parking at

various locations around the building. This level of provision is in accordance with published standards for cycle parking in Cambridge, but it would be preferable for all the cycle parking to be covered if possible.

Financial Contributions to the Southern Corridor Area Transport Plan and the Cambridgeshire Guided Busway scheme

Within the draft Section 106 Agreement, financial contributions are proposed towards the Southern Corridor Area transport Plan (SCATP) and towards the Cambridgeshire Guided Busway, although no specific sums are mentioned. The principle of such contributions can be justified as the reduction in on-site car parking will mean that the development will place greater emphasis upon local bus services, the Cambridge Park and Ride scheme, and the guided Busway services to provide accessibility to the site.

The following contributions have been offered.

Element of development	SCATP contribution	CGB contribution	Total
50 Station Road	£96,988	£344,233	£441,221
60 Station Road	£116,757	£414,397	£531,154
Total	£213,745	£758,630	£972,375

These levels of contribution are acceptable to the County Council, as they make due allowance for deferred payments from other elements of the CB1 development in line with previous discussions. The recommended trigger points are prior to commencement of each element of the development.

Workplace Travel Plan

A Travel Plan has been submitted for the development, and in general this includes the expected range of measures for a framework Workplace Travel Plan. Some measures that could be considered to further enhance the WTP are the introduction of cycle training and bike buddy initiatives to help promote cycling to less confident cyclists, and the introduction of real-time passenger information into the offices themselves, potentially linked to the RTP1 signs at the railway station and the

bus interchange. This would help commuters and visitors to check whether their bus or train were running to time before leaving the office. A screen showing real-time rail and bus departures, for example, could be installed in the entrance lobby of each building.

The framework WTP includes mode shift targets of a 5% shift from car driver to public transport over a five year period, and a 5% shift from car driver to walk and cycle over the same period. This requires a baseline to be identified, which will need to be done through survey. A more meaningful approach to target setting might set these targets against the parking availability at the office. For example, the 76 spaces are expected to serve around 1,140 employees on site, meaning that a maximum of 6% of employees could drive to work and park on site (76 car drivers out of 1,140 employees), 18% could car share (maximum of 3 people per car excluding the driver), and 80% will need to reach the site by public transport, park and ride, guided Busway, on foot or by bike. These percentages could set the baseline for the WTP targets by establishing a maximum number of car journeys that could be expected at the site.

The framework WTP should also set out measures to manage business mileage, such as the provision of pool cars on site, which would need to be allowed for within the 76 car parking spaces on site.

Head of Environmental Services

6.3 *Construction/demolition*

During construction and demolition, noise, vibration and dust has the potential to harm the locality amenity if not controlled. A Demolition and Construction Environmental Management Plan (DCEMP) should be secured by condition.

Acoustic Report - Road Traffic Noise

It has already been identified that Station Road is and will remain noisy. Satisfactory noise levels can be achieved with sealed double glazing. A full ventilation and glazing scheme is required and this can be secured via a noise insulation condition.

Acoustic Report - Plant Noise

The details of the plant and therefore its noise are not finalised and the standard plant noise condition is recommended for discharge "Prior to occupation".

Odour

Depending on the use of the retail café / restaurant area odour may harm the amenity neighbouring homes and offices. The standard odour control condition is recommended.

Opening Times (retail, café, restaurant)

A condition to restrict opening hours/deliveries to 07:00-23:00 is recommended.

Car Park Ventilation

The ventilation of the underground car parks on other phases of CB1 has previously been considered as part of the planning process. However, it has now been confirmed that this is covered by the Building Regulations.

Contaminated Land

The development is for the demolition of the existing buildings and the erection of two office blocks (known as Numbers 50 and 60 Station Road). The site, part of CB1 Development, was investigated during the Foster Mills redevelopment and the submission of the outline application in 2006.

A comprehensive desktop study was undertaken and noted multiple past industrial uses on and off the site including scrap yards, laboratories, fuel tanks and flourmills. The intrusive investigations recorded elevated concentrations of ground gases (up to 12.4% v/v of carbon dioxide –minimal flow rate was noted) and made ground impacted by aromatic hydrocarbon contamination.

Contamination issues were adequately assessed for the eastern half of the site (known as 60 Station Road) during the Foster Mills development. No further investigation is required in this area.

Limited investigation so far has been undertaken around the western part of the site (currently occupied by Numbers 32-38 Station Road). Further investigation is required in this area following site clearance and this can be secured by condition.

Waste Strategy

Further details of bin store plans and layout and use of compactor are needed. A trade waste condition is recommended.

Air Quality

Restriction of car parking is an important tool in the management of traffic levels and congestion, thus helping to improve or to prevent the deterioration of air quality. The level of car parking is satisfactory on air quality grounds

Licensing and Food and Occupational Safety

Informatives are requested.

Urban Design and Conservation Team

6.4 *Application as submitted*

Comments on the Planning Application

Overall conclusion:

The application is supported subject to clarification regarding proposed materials.

Amendments are required to the northwest corner at the ground floor and to improve activity and surveillance along Warren Close.

Additional amendments are required to the top of the 6th floor of No50 Station Road to introduce the same detailing as on the adjacent No60 with the stone columns extending upwards to help screen the 7th floor and plant enclosure.

Existing Buildings:

The existing terraced houses (32-38 Station Road) are Buildings of Local Interest (BLI), in some ways, are pretty typical of their type and era and common enough in many larger towns and cities across Britain. These examples are slightly more decorative than is usual in Cambridge, with the 'crow-stepped' gables and red brick banding, quoins and so on. This may result from the locally well-known architect, Richard Reynolds Rowe, who did use such details and operated in the area and, if this could be indisputably proved, would give rather more weight to the history of the terrace. Nonetheless, these houses are not particularly rare architecturally and have lost some of their residential character through changes-of-use, particularly by the unfortunate and visually prominent ramp occupying the front garden space of the surgery. The other item of interest is the 'no fines'-type concrete boundary wall that also occurs in front of the villas further down Station Road and again in Warkworth Street & Terrace nearby. This is believed to be a very early use of concrete but not enough research has been done to establish the rarity or historic value of these examples.

Proposals:

A previous application (12/0502/FUL) was refused at Committee despite officer support for that scheme.

UDC comments relate to Reason for Refusal 1. Views relating to Reason for Refusal 3 are contained within the Conservation Area Consent comments. The other two reasons are not directly related to the design and conservation issues. However, the scheme has been amended to accommodate additional basement car parking without unduly compromising the scheme. There are some concerns relating to the resolution of the ground floor cycle parking.

As with the previous scheme, the applicant has undertaken an assessment of the proposals against the approved 'Guidance for the application of Policy 3/13 (Tall Buildings and the Skyline) of the Cambridge Local Plan (2006)' contained within the submitted Design & Access Statement. As with the previous scheme, the UDC team are satisfied with the assessment of the proposal against the guidance and support its conclusions.

The proposals have not changed significantly in terms of the proposed scale and massing. Instead the applicant has moved the Station Road building line of No. 50 back by approximately 2m and amended the top of the 6th floor to drop the perceived height of this part of the building. This has been achieved through the removal of the extended stone columns (as seen on the adjacent No. 60) to reduce the top level of the 6th floor.

The Design & Access Statement contains a number of the perspective views that show how the proposals fit into the CB1 masterplan. Looking east along Station Road, the effect is to promote the prominence of No. 60 on Station Road, allowing it to take on more of the role envisaged for I2 in the Outline Masterplan. The entrance 'pod' between the two buildings is more visible and therefore aids the legibility of the entrance when viewed looking east towards the Station.

The view of the UDC team is that the amendment to the building line is less successful when looking west down Station Road. The previous scheme read as two distinct forms that helped to articulate a sense of rhythm along the street and helped to break down the perceived massing of the buildings. The revised scheme does not create this sense of articulation due to the recessed No. 50 appearing to merge with the more prominent No. 60 (pages 34 and 67 of the D&A Statement).

Overall the view of the UDC team is that the scale and massing is very similar to the previous scheme, which was supported in Design & Conservation terms. Given that this scheme is so similar to the previously refused scheme on balance, the UD C team view is that it is supportable in Design & Conservation terms. The 'stepping' of the building line by moving No50 back does not radically change the perceived scale and massing of the proposals overall, and in particular from Station Road.

Other comments

Ground floor plan and western elevation

There is a conflict between the column on the western side of the building and the wall around the proposed cycle store due to the addition of the wall to the cycle parking. This wall creates a poorly configured internal space and conflicts with the structural

columns of the building that will be visible from the street outside.

The plans show that the northwest ground floor 'corner' of No50 will be glazed and this will mean that the wall to the proposed toilets will be visible behind the glazing. This needs to be reconsidered.

The western ground floor elevation of this scheme is very blank in appearance and also needs to be reviewed.

Sixth floor

The previous scheme created a consistent approach between the top of the stonework on both 50 and 60 Station Road with the columns extended up beyond the top of the floor plate above to help screen the plant floor above. As proposed, No. 60 maintains this approach but the stonework for No. 50 is truncated. The view of the UDC team is that the consistent approach worked more successfully and provided a more defined edge to the top of this element of the building. As proposed, No. 50 has a somewhat confused 'top' by revealing the 7th floor materials and then the plant enclosure above.

The stone columns at the sixth floor should be extended upwards, as on the seventh floor at No. 60, to create a more resolved top to No. 50 Station Road (page 44 of the submitted D&A Statement illustrates this approach). Extending this approach onto the western and southern elevation would improve the composition of the junction between the sixth and seventh floor when viewed looking east along Station Road and north through the park.

Plant level

The plant level on No. 50 is more visible than with the previous scheme due to the amendment to the top of the 6th floor that has removed the screening quality of the stonework to reveal more direct views to the plant level.

Materials

No details of the proposed materials are on any of the drawings submitted for approval. A schedule of materials is therefore

required that identifies their use and position on the proposed elevations.

Assessment against Reason for Refusal 1

The stepping back of No. 50 does little to change the perceived scale and massing of the proposals on Station Road and increases the perceived bulk of the buildings when looking west due to the removal of the gap between them and therefore removing the articulation of the overall form. The modification to the top of the 6th floor removes the screening to the 7th floor resulting in the junction between the top of the 7th and the plant floor becoming more visible. The UDC team suggest that an option that extends the stone columns at the 6th floor upwards on No50 (as on the 7th floor at No60) to create a more resolved should be explored.

In addition, further clarification of the northwest corner is needed along with a detailed materials schedule, which appears to be missing from the submitted drawings.

Additional comments on the application for Conservation Area Consent

The applicant's agents have submitted a Heritage Statement, which covers the history of the area in some detail and analyses the 'significance' of the various components of the BLIs and the CA. There is broad agreement with the content in those respects.

This application relates to a replacement building of a slightly different design to the previous application, so the discussion of the proposal to demolish the existing terrace to clear the site remains very similar to the previous.

"Whilst the 'masterplan' process always assumed demolition of these BLIs, no consent has ever been granted and the changed circumstances since 2008 and changes to the proposals for this area of the larger scheme mean that this CAC application must be assessed against current policies. It also is tied inextricably to the application for the replacement building and the two things must be seen, assessed and balanced together. The houses are undoubtedly 'heritage assets' although of fairly modest significance in themselves and changes to the

surrounding area in recent decades have disconnected them from the nearby residential areas of similar age and scale. They were also in the same area as, but of a different character to, the railway lands & buildings nearby [and the industry co-located deliberately] and hence the station building [the LB itself]. Once the character of the area was changed substantially by the demolition for and construction of the so-called 'Deity' office blocks, the residential nature became confined to the other side of the street largely and the gradual drift of industry away from the now mainly passenger railway allowed for the comprehensive redevelopment proposed by the 'masterplan'. Whilst the substantially different character to the area already being formed [by the Microsoft HQ building, for example] will contrast strongly with these BLIs, that is not – in itself – a reason for demolition; indeed the opposite might be argued as the whole scheme is likely to take years to complete and there will be times during redevelopment that noticeable contrast in building ages, types and scales will be evident. And there will be, or should be, a marked difference in age, type and scale between the LB and its new setting for the foreseeable future and nobody seems to have suggested that was inappropriate."

However, the 'masterplan' always envisaged this side of the street to be commercial in nature and of a much larger scale and that intent would have been fully aired at the 'outline' application stage. The terraced houses are not of great architectural or historic significance although they have a pleasant, modest character but are not particularly unusual in the wider context. So the policies requiring justification for demolition in CAs need to be tested against the 'public benefit' from redevelopment

If the BLIs are to be demolished, then the quality and suitability of the proposed replacement building(s) both for the CA and the nearby LB must be assessed before any scheme can be supported. The detailed assessment by U D & C Section is to be found at 12 /1556 / FUL on this matter.

Conclusion:

The loss of BLIs in a prominent location in the CA is always a matter for regret and the policies at national & local level are against such a loss except in cases where there is a

demonstrable benefit to be derived. In this case, the decision makers who granted outline permission for the 'masterplan' were aware of the presumed demolition and assessed it to be worthwhile. All the discussions about the replacement building were predicated on the design having to be assessed as of suitable quality to meet the policy 'tests'.

Suggested Conditions

Conditions are recommended to address the following:

1. stonework details
2. non-masonry walling systems
3. glass types
4. joinery
5. low-pitched roof details
6. roof glazing system
7. renewable energy source(s)
8. rooftop plant screening system
9. coping to the walls
10. metalwork
11. colonnade soffits.
12. locations for signage systems
13. masonry support systems.
14. photographic record

6.5 *Response to additional information*

The applicant has made the following amendments to the scheme in response to our comments:

- 1) The stone columns at the 6th floor have been extended upwards to match the detail on No.60.
- 2) A discrepancy between the plan and elevation on drawing 101 has been resolved. The wall indicated on this elevation will be solid and not glazed.
- 3) The ground floor fronting on to Warren Close has been reconfigured to provide office frontage as opposed to part cycle store and part office.

In our view the amendments address all of the concerns raised and are therefore supported in Urban Design & Conservation terms.

Senior Sustainability Officer (Design and Construction)

6.6 *Application as submitted*

Policy Context

The masterplan for the site contains the ambition for all development to exceed Part L of the Building Regulations by 10%, and for office developments to utilise PV and Ground Source Heat Pumps to achieve a 15% abatement of carbon emissions from renewable energy systems. While there are many elements of the proposals that are very encouraging and in keeping with the ambitions of the CB1 Masterplan, there are other areas of the proposals that are a little disappointing, particularly in relation to the role of renewable and low carbon energy generation.

Sustainable Development

The majority of the approach being taken is supported, including the specification of a green roof for the scheme, which should have multiple benefits. The approach to reduce water consumption is also supported, although given the severe water stress faced by the city, the applicant should go as far as possible in reducing demand for water. The scheme is also targeting BREEAM 'excellent' and the BREEAM New Construction 2011 Planning Statement sets out the approach to targeting credits. A score of 72.27% is predicted if all possible credits are achieved. This approach is fully supported.

Reduction of Carbon Emissions and Renewable Energy

Following a hierarchical approach to reducing carbon emissions, the proposals should lead to a 32.2% reduction in carbon emissions overall compared to a Part L 2010 compliant development (based on regulated emissions only). This hierarchical approach to reducing emissions, and the level of reduction being achieved is fully supported.

With regard to meeting the requirements of Policy 8/16, three approaches/technologies are to be utilised, namely:

- Passive solar design measures to reduce emissions by 6.08% (21.4 tonnes CO₂ per annum);

- A 30m² photovoltaic array to reduce emissions by 0.63% (1.5 tonnes CO₂ per annum); and
- Solar thermal panels to reduce emissions by 2.12% (2.6 tonnes CO₂ per annum).

These three techniques combined lead to an 8.83% reduction in CO₂ emissions.

While the combined use of passive design measures, photovoltaic panels and solar thermal technology is slightly below the Council's policy requirement, when set against the overall levels of carbon reduction being achieved the approach being taken is considered to be acceptable.

The combined use of a green roof and photovoltaic panels is also welcomed, as the green roof should help to maintain a stable microclimate around the panels, helping them to operate efficiently. Recommendations to incorporate routes and sleeves into the design of the development to enable future connection to district heating is also supported, given the proximity of the scheme to the heat network serving the student accommodation on the Station Area site. This could provide a future source of low carbon heat and electricity to the scheme.

Access Officer

6.7 Comments on previous application:

The main door this should be automated. Side doors alongside revolving doors would best electrically opening or asymmetrical of which one is at least 900mm.

Reception and cafe bars need dropped height count and hearing loop.

Head of Streets and Open Spaces (Tree Team)

6.8 No comments received.

Head of Streets and Open Spaces (Landscape Team)

6.9 Public Realm

Cycle parking along Station Road and the SAR has been reduced but the location of spaces in the northeast corner still presents a barrier and should be moved to the south.

Visual Amenity Space

The green roof over the retail pavilion between 50 and 60 Station Road is welcomed and when viewed from Station Road will create an interesting and contrasting green edge to the built form. We would however suggest that this should extend the full length of the space between the two buildings to meet the usable amenity space towards the southern elevation. This would substantially improve the visual amenity for the neighbouring offices, eliminating views of the unsightly car park ramp and the cycle parking.

Useable Amenity Space

The green roof over the retail pavilion between 50 and 60 Station Road is welcomed and when viewed from Station Road will create an interesting and contrasting green edge to the built form.

The scheme proposes trailing plants on wires between the green roof and the amenity space to help screen the car park ramps from view, which is in accordance with the CB1 landscape strategy. The proposal is not practical as the climbing plants cannot be tied on, or managed in any way. Another portion of green roof should be added to completely cover the ramps. Also the proposal of trailing plants on wires between the green roof and the amenity space fails to take the opportunity to create a meaningful amenity space on the first floor.

Balconies on the southern elevation of Number 60, could also provide some amenity space for each floor as well as activate the elevations overlooking the 'Ante chamber' square.

Green/Brown roofs

The inclusion of green roofs is welcomed. Specifications and the performance specification of the brown roofs are needed.

Conclusion

Minor improvements have been incorporated into the proposal, which includes; an increased legibility of pedestrian space around the entrance to the car park and the replacement of 'pleached trees' with larger specimen trees.

The proposed development has the potential to make a very positive contribution to the Station Area with the capitalisation of the opportunities identified above. Subject to the incorporation of the suggested amendments, the proposed development of 50 & 60 Station Road can be supported on landscape, visual and amenity grounds.

Head of Streets and Open Spaces (Walking and Cycling Officer)

6.10 Application as submitted

Ground level cycle parking is welcomed. Access to cycle parking at 50 Station Road is not convenient given two doors to negotiate. All doors should be at least 900 mm wide. The cycle lane crossing the service area and the layout of cycle parking in the service area needs to be amended.

Response to additional information

Further comments to be provided on the Amendment Sheet or orally at the Committee meeting.

Head of Streets and Open Spaces (Sustainable Drainage Officer)

6.11 No comments

Head of Streets and Open Spaces (Nature Conservation Officer)

- 6.12 The Report on Ecology prepared by RPS (dated Nov 2012) accords with the site wide ECMP This would seem a logical approach for this application and is supported.

The proposed green and brown roof provision significantly increases available habitats in this relatively dense urban environment.

There is an error in the document in that they cite that artificial roost boxes will be available for bats. However section 3.12 rules this option out 'artificial site aren't provided for bats within 50 and 60 Station Rd'. I am satisfied that the building materials may prohibit such installation but clearly the document needs to be consistent. (Note: the document has been updated)

The additional document 32 – 38 STATION ROAD, CAMBRIDGE, CB1 Inspections of properties with bat roost potential provides details of an appropriate follow up survey for the potential of bats using the buildings proposed for demolition. The recommendations proposed in the conclusion are supported.

English Heritage

- 6.13 Comments as previous applications:

Summary

The principle of demolition of 32-38 Station Road was given tacit support when the CB1 Masterplan was agreed. However consent should only be granted once a scheme for high quality replacement buildings has been secured. The current proposals require changes to the Masterplan which have much to recommend them and the revised design has gone a long way to address the previous concerns raised by English Heritage.

Conservation Area Consent application

The NPPF includes a provision of a presumption in favour of the conservation of heritage assets. In this instance 32-38 Station

Road are not designated heritage assets but the Conservation Area within which they are located is and therefore the presumption in favour of conservation is relevant. The removal of these buildings will enable the implementation of a holistic redevelopment that will deliver an overall enhancement of the Conservation Area whereas to retain them would be at odds with the new context.

Construction of New Office Building

The office building has the potential to deliver an improvement on the Masterplan. In particular it allows the creation of a new area of public realm as an 'ante chamber' to the Station Square. However the full potential of this space cannot be realised until the future of the Silo building is resolved.

The revised massing strategy and the vertical emphasis that is delivered by the expressed stair to 60 Station Road is broadly welcomed.

The revised design that now incorporates a series of 'giant order' of mullions and transoms finished in reconstituted stone references the Cambridge context. The details will need to be strongly controlled to avoid problems with staining.

The lack of a double height recess on 50 Station Road (south elevation) makes this building appear 'stunted'. The plans could be revised to address this.

There is no detail of signage locations.

Recommendations:

- ☐ Review South elevation of 50 Station Road
- ☐ Approve with conditions

The Victorian Society

- 6.14 The Victorian Society was not formally consulted on the application because 32-38 Station Road are not listed buildings. No comments have been received in relation to the current applications. The following comments were made previously:

The Society objects to the demolition of 32-38 Station Road. They are a striking late Victorian terrace and may have been designed by Richard Reynolds-Rowe. The rarity of the buildings adds to their value in the streetscape.

The poor state of the frontage and the access ramp can be easily rectified. Other sites could provide new office space that would negate the need to demolish these buildings.

The Council has an obligation to ensure that development either preserves or enhances the Conservation Area.

Natural England

- 6.15 Welcomes development, which incorporates sustainable design and construction and shows both adaptation to and reduction in the contribution to climate change.

Detailed comments:

Support the Ecological Assessment particularly the need for involvement of a licensed bat ecologist and action to deal with nesting birds.

Monitoring should be carried out to ensure that the proposed development has been successful in providing habitat for both birds and bats.

Environment Agency

- 6.16 Conditions recommended to address groundwater and contaminated land issues, surface water disposal, piling and groundwater, surface water drainage and pollution control. Informatives are requested regarding surface and foul water drainage, pollution control and other legislation.

Anglian Water

- 6.17 No comments.

Ministry of Defence (Air Safeguarding)

- 6.18 No comments.

Cambridgeshire Constabulary (Architectural Liaison Officer)

- 6.19 No comments on current application. Previous comments are as follows:

All entrances beyond main reception and other entry doors (especially those from the basement) should be on card swipe/key fob. CCTV should be in place covering the basement and circulation areas of the building and in particular cycle parking area.

In terms of crime risk the underground car park and cycle parking for the scheme are issues. More information is needed about how the basement car park is to be secured 'out of normal' operating hours.

There is a problem with cycle crime and cycle parking around the railway station and any open cycle parking in the area will need to be strictly managed and abandoned cycles culled. A notice to the effect that abandoned cycles will be removed should be clearly visible. There are a number of Sheffield Hoops located around the perimeter of the building. During normal offices hours these will be the subject of good surveillance but out of hours there will be less opportunity for surveillance.

The secured cycle parking should not be accessible from the top of the car park ramp.

(Note – the applicants have confirmed direct to the ALO that access is secured by swipe card/key fob and CCTV is to be provided. The ALO is satisfied with this explanation).

Cambridgeshire County Council (Archaeology)

- 6.20 No comments received.

Design and Conservation Panel (Meeting of 14 March 2012)

- 6.21 The conclusions of the Panel meeting were as follows:

In strategic terms, the Panel considers that the new approach is a great improvement. The change in massing, the handling of

the frontage at ground level along Station Road and the greater animation of the frontage to the 'anti-chamber' square to the south are welcomed. The 'kit of parts' proposed for the elevations looks promising but further refinement of the design is still needed, as is further examination of the treatment of stair cores at ground level.

VERDICT –

1. The strategy of the massing, the relationship of the stair core with Station Road, the overall strategy for the elevations and the handling of the public realm, GREEN (5), AMBER (1)
2. The 'kit of parts' for the elevations, the handling of the elevations at ground level, the design of the 'fins' and other components and the planting of the terraced area, GREEN (3), AMBER (2)

The relevant section of the minutes of the panel meeting(s) are attached to this report as Appendix A.

Design and Conservation Panel (Meeting of 13 February 2013)

6.22 The minutes of the Panel meeting were as follows:

Following the last presentation in March 2012 and the refusal in July of the previous application (12/0502/FUL), the Panel were invited to review the amendments made to the scheme, which is the subject of the current application. The amendments include; the pushing back of the facade of No 50 to reduce its apparent bulk and the inclusion of an additional 15 car parking spaces at basement level. The Panel were told that the application is to be recommended for approval at Planning Committee in early March.

The Panel's comments are as follows:

- ☐ Set back of No. 50. The Panel welcome the setting back by 3 metres of the facade of No.50. It was felt that this new configuration had several advantages over the previous flush alignment of the two buildings. The benefits arising from the adjustment to the building's alignment were felt to include:-

- o the introduction of an element of ascending progression injected more interest to the streetscape and sets off the café space more effectively,
- o had created more 'breathing space' at pavement level which could lead to less potential conflict between pedestrians and cyclists,
- o allowed for more flexibility regarding the choice of street trees
- o the generation of an 'implied' space shared with buildings on the opposite side of Station Road.

☐ Stonework fins. In the Panel's view, the proposed application of the simple components, sometimes projected, rotated and flush was playful and created interest. However, the Panel commented that careful detailing is necessary to prevent the kind of unwelcome shadowing caused by the accumulation of dirt between the joints.

☐ Entrance (No 50). The Panel are comfortable with the logic of the new entrance. The addition of an etched '50' into the stonework would, in the Panel's view, introduce a playful typographical element and enhance the entrance.

☐ Additional parking spaces. The pressure to provide additional car parking in order to make the buildings more marketable is understood, and the Panel welcome the determination to deliver the extra spaces without compromising the design.

☐ Potential wider benefit arising from the demolition of Wilton Terrace (32-38 Station Road). Within the terms of the National Planning Policy Framework, the Panel felt that the removal of the heritage asset is justified as it would enable the implementation of the CB1 Masterplan as a whole, and enhance the Conservation Area.

Conclusion.

The subtle changes deliver a significantly enhanced scheme. A better relationship between the two buildings and the adjoining space has resulted from the move away from them being a

matched pair to becoming a *related* pair. The retention of 32-38 Station Road was felt to be at odds with the new context set by the implementation of a holistic redevelopment.

VERDICT – GREEN (unanimous)

The above responses are a summary of the comments that have been received. Full details of the consultation responses can be inspected on the application file.

7.0 REPRESENTATIONS

- 7.1 The owners/occupiers of the following addresses have made representations:

22 Brooklands Avenue
5 Clarendon Road
9 Clarendon Road
17 Clarendon Road (x4)
33 Glisson Road
2 Highsett, Hills Road
6 Highsett, Hills Road
40 Highsett, Hills Road
46 Highsett, Hills Road
60 Highsett, Hills Road
61 Highsett, Hills Road
62 Highsett, Hills Road
69 Highsett, Hills Road
75 Highsett, Hills Road
82 Highsett, Hills Road
59 Mawson Road
21 Panton Street
2 Saxon Street
15 Shelly Garden
25 Tenison Avenue
10 The Lawns, Clerk Maxwell Road
2 Vintner Terrace
1 Warren Close
Whitlocks, High Street, Trumpington

- 7.2 The representations can be summarised as follows:

Concerns and objections

Previous proposals and current applications

The proposals are not substantially different and the revisions make no difference.

Both applications should be refused.

Loss of Existing Buildings

The existing buildings with their architectural heritage and elegant facades should not be demolished.

The existing buildings are an important example of an earlier age of fine architecture, which should be preserved.

Wilton Terrace has been designed to relate to the Station.

Demolition of the existing buildings amounts to vandalism against the wishes of residents.

Loss of welcoming vista along Station Road.

The terrace does not need to be demolished and could be incorporated into the new development.

Renovation of the buildings would be more sustainable.

Have planners relied on reports on architectural value of Wilton Terrace by the applicants or have they commissioned an independent view?

The terrace will not clash with other buildings but will provide a historically relevant backdrop.

The New Buildings

The buildings are still too large in all dimensions particularly height.

Offices of eight floors do not add to the visual amenity of Cambridge. Visitors come to the city for its character and do not want to see bland office blocks.

The buildings are too large and dominant and will have an adverse impact on the character and open spaces in the immediate area.

The buildings are too large and dominant near to historic, conservation and residential areas.

The buildings will have a negative impact on the listed Station and Mill and Station Road.

The new buildings are not appropriate in Cambridge and should not be supported.

The development will be a first impression for thousands of visitors.

Other new buildings do not fit in with the character of the surrounding area.

Other new buildings are already out of scale with the Victorian villas on Station Road.

The new buildings are more appropriate for Milton Keynes.

The drawings give the impression of the building being in open space when it will have an overbearing impact on a narrow street.

The overall design of Station Road is becoming piecemeal.

There is a rule that no buildings in Cambridge should be higher than Kings College Chapel. Has this been forgotten?

The development will adversely affect Highsett leading to devaluation and potential redevelopment.

Other Issues relating to Trees, Amenity and Parking

Several beautiful trees will need to be removed.

The level of car parking has been increased but is still inadequate.

The buildings will generate more traffic close to the Station where arrangements for pick up and drop off are becoming increasingly chaotic.

Cycle parking should be made here for people using the Station and new buildings in the area. Another issue within this location is a lack of dedicated motorcycle bays within the train station location for use by commuters.

There is an excess of office accommodation in the City and little justification for more.

The argument that the development supports employment objectives is doubtful since jobs would be created if the buildings were renovated.

The move by the government to relax planning controls should be resisted.

The implied threat that the Station Cycle Park may not go ahead if this application is turned down is 'disgraceful'. The Cycle Park should be delivered anyway and not appended to this planning application.

Procedural Issues

Full planning permission can only be sensibly considered in the light of a revised Masterplan, so that the development as a whole would conform to the parameters agreed at Outline. To do otherwise makes a 'nonsense of the planning system'.

7.3 Brooklands Avenue Area Residents Association has made representations as follows:

The application represents over-development in both height and mass.

A separate full application should not have been submitted rather than a Reserved Matters application. If the full application were to be agreed it would create an unwelcome precedent, and might render the original scheme, agreed at outline stage, redundant.

An Environmental Impact Assessment should be required. The application is not a Reserved Matters application: it is a full stand-alone application and must be viewed entirely on its own merits and not with reference to the CB1 Masterplan agreed in outline.

A full Traffic Impact Assessment is also required; as is consideration of other matters such as car parking, open space provision, accessibility to emergency vehicles and waste collection and demolition of existing buildings.

Height of buildings

Both the old Foster's Mill and the Station itself would be adversely affected by the erection of two massive "glass box" office buildings in such close proximity. The Mill is a significant part of the local skyline and the proposal would breach the policy 3/13. It would also dominate the architecturally important and historic station building, which has two storeys.

Demolition of existing buildings

We object to the demolition of the present Victorian terrace on the site (BLIs), and believe that it should be preserved, as architecturally in keeping with the properties on the other side of Station Road and of the Station itself. In this connection we further note that the houses are designated as "Buildings of Local Interest." As indicated above, the case for demolition cannot be deemed as having been agreed, since as a full application this has to be considered from scratch, and cannot ride on the back of the Masterplan outline. Indeed there is a strong argument for retaining the properties in the interest of balance with the similar buildings on the north side of Station Road, and thereby preserving important features of this part of the Central Conservation Area.

Car and Cycle Parking

There is space for 1000 workers in the building. Car parking provision is inadequate and only 576 cycle parking spaces are proposed. The rest of employees will have to come by foot or public transport.

Comparison with earlier refused applications

The height and massing of the proposed buildings would make them unduly dominant – this still applies especially to number 60, which is unchanged and continues to detract from the Mill. Number 50 is slightly smaller but both buildings dominate the Microsoft Headquarters. There is not attempt to differentiate between the facades of each floor, which Wilton Terrace illustrates well.

Parking provision is inadequate – There has been a 25% increase in car parking and 33% increase in cycle parking but provision is still inadequate.

Insufficient clear public benefit to compensate for loss of a heritage asset – note ‘clear public benefit’ and not ‘clear financial benefit to the developer’. No such clear public benefit has been demonstrated.

For all the above reasons we urge the City Council to reject the proposed applications.

7.4 David Campbell Bannerman MEP has also made objections that relate to the loss of 32-38 Station Road. He objected to the previous application and does not believe that the application has notably changed. Following the previous Committee meeting he thought that a compromise was possible to include the retention of the building and that several Councillors agreed with this. He considers that the loss of these buildings and the new development to be harmful to the Conservation Area. He considers that the existing buildings have an evidential, aesthetic and communal value. In his view the level of harm caused by the loss of 32-38 Station Road heavily outweighs the benefits gained from the replacement building.

7.5 The above representations are a summary of the comments that have been received. Full details of the representations can be inspected on the application file.

8.0 ASSESSMENT

8.1 From the consultation responses and representations received and from my inspection of the site and the surroundings, I consider that the main issues are:

1. Principle of development
2. Environmental Assessment
3. Context of site, design and external spaces
4. Impact on Heritage Assets
5. Public Art
6. Renewable energy and sustainability
7. Disabled access
8. Residential amenity
9. Refuse arrangements
10. Other environmental impacts
11. Transport Impact
12. Highway safety
13. Car and cycle parking
14. Third party representations
15. Planning Obligation Strategy

Principle of Development

- 8.2 The application has two key parts; demolition of 32-38 Station Road and the erection of a pair of office buildings. The approved Parameter Plans that form the Masterplan for the Station Area redevelopment include both of these elements and in my view establishes the principle for the development.

Demolition of 32-38 Station Road

- 8.3 Parameter Plan 1 of the Masterplan indicates 32-38 Station Road as a group of Buildings of Local Interest that are to be demolished. In the report that was considered by Planning Committee in October 2008 the following comment is made about the demolition of these buildings:

‘The applicant has not provided a full justification for the demolition of 32-38 Station Road that will be necessary when an application is made for Conservation Area Consent for its demolition. Clearly Block I2 cannot be developed without removing 32-38 Station Road. The pivotal role that I2 has in the scheme is considered more fully below but essentially I would conclude that the loss of 32-38 Station Road is justified by the need to increase the density of development across the site in order to achieve the aim of improving the transport interchange. 32-38 Station Road are not worthy of listing and in my view to seek to refuse the masterplan on the grounds that these

buildings should be retained alone would be very difficult to substantiate at appeal.'

- 8.4 My views have not changed. I consider that subject to the grant of Conservation Area Consent, the principle of the demolition of 32-38 Station Road is acceptable. I have addressed the issue of demolition in greater depth in the following section 'Impact on Heritage Assets' and in my report for the Conservation Area Consent. The CAC report also refers to the more robust response that the applicants have provided in relation to the public benefits that will accrue from redevelopment of the site.
- 8.5 The demolition of 32-38 Station Road will also result in the loss of the doctor's surgery that occupies part of the building. The loss of such a community facility is contrary to Policy 5/11 of the Local Plan unless the facility is replaced as part of the development, is to be relocated to an equally accessible location or is no longer needed.
- 8.6 There is a continuing need for the surgery and it is not relocated, as part of the development therefore the only way in which this policy objection can be overcome is to secure the relocation of the surgery. This was the option that was pursued by the Outline consent and it was secured by the s106 Agreement. I would recommend that the s106 Agreement for this site be similarly worded to secure relocation of the surgery prior to the commencement of 50 Station Road. I am making this recommendation notwithstanding the fact that it is likely that the surgery will be relocating to a new site shortly.

The New Office Buildings

- 8.7 The Outline approval for the Masterplan shows Block I2 as an office building with the potential for retail use along the North (Station Road) and East (SAR) elevations. The proposed development accords with this disposition of uses within a pair of buildings that have a larger footprint.
- 8.8 The quantum of development is much greater than for Block I2 because the footprint of the site is larger including land which was previously going to form the SAR and part of the adjacent Block I1. The accommodation schedule for the Masterplan shows an indicative floorspace of 11506 sq m for Block I2 that compares with 15900 sq m now proposed. Whilst it is important

to remember that this is a freestanding application the floorspace that is proposed does not exceed the overall office floorspace permitted by the Outline Consent.

- 8.9 In my view the office/retail use and the quantum of floorspace proposed are acceptable in principle. My assessment addresses the impacts of the office development in greater depth.

The Fall Back Position

- 8.10 In considering the principle of development it is also important to have an awareness of the 'fall back position'. In this case if planning permission is refused the applicants could revert to the Outline Planning Permission and bring forward a 'reserved matters' submission. If this submission accorded with the constraints set out by the Parameter Plans including the 75% build out of Block I2 it would be very difficult to justify a refusal. Such an application, if approved would result in the construction of a building of radically different scale to the prevailing character of the Station Area prior in 2008 but one which would be compatible with the buildings currently under construction. In my view, given the fall back position, it would be sensible to focus consideration on whether the design and appearance of the building is appropriate to its context and not on whether a nine storey building is acceptable in principle.

Restriction on occupation of office development

- 8.11 Policy 7/2 of the Local Plan permits new office development for occupation by a business that can demonstrate that it provides an essential service for Cambridge as a local or sub-regional centre or exceptionally where there is a proven need for a regional function only. This is sometimes known as a 'local user condition'. The s106 Agreement for the outline application secures such control over the future occupation of office development within the scheme and it is necessary to secure the same arrangement for this proposal. This can be achieved through the s106 Agreement.
- 8.12 In my opinion, the principle of the development is acceptable and in accordance with policies 3/1 and 9/9 of the Cambridge Local Plan 2006.

Context of site, design and external spaces

- 8.13 In my view the following issues are of relevance to this part of my Assessment, where appropriate I have highlighted the differences between the current scheme and the earlier scheme:

The Masterplan, Parameter Plans and the Design and Access Statement for the Outline application

Footprint of the building in comparison with approved Masterplan

Building Design – Layout

Building Design – Height

Building Design – Elevations and Materials

Building Design – Phasing

External Space – Realignment of the SAR

External Space – Design and Materials

External Space – Tree removal and planting

Nature Conservation

The Masterplan, Parameter Plans and the Design and Access Statement for the Outline application

- 8.14 The development is not constrained by the approved Parameter Plans in the same way as applications for reserved matters. However in assessing whether or not the design of the building is appropriate for its context it is useful to consider the Parameter Plans and the assumptions that underpin them. The parameter plans set a threshold on matters such as the footprint and height of the blocks as they come forward in their detailed form and the approvals of the parameter plans were predicated on the assumption that buildings of such height and mass would be acceptable in the context of the site.

8.15 The key Parameter Plans that relate to the design of buildings and spaces are:

- ❑ PP3 Building Layout (+ maximum balcony/canopy overhang 1.5m)
- ❑ PP4 Building and Ground Conditions (building height (maximum height of occupied floorspace + maximum plant/lift motor rooms 2 m), building height above proposed ground level, proposed ground level (+/- 0.5m tolerance), existing ground level and proposed ground floor setback)
- ❑ PP6 Public Realm and Open Space

These Parameter Plans were informed by the Design and Access Statement that was submitted to support the Outline application.

8.16 Block I2 that is approved for this part of the Masterplan was always intended to be the largest new block in the scheme. In the report that was considered by Planning Committee in October 2008 the following comment is made about Block I2:

‘Part of the argument in favour of a tall building on Block I2 is that in order to fulfil the key aim of providing for a high quality transport interchange a certain level of development will need to be brought forward to fund such a facility. Insurmountable constraints in terms of the setting of the listed Station buildings, the Mill and Silo, the desire to create a civic space in front of the Station surrounded by buildings of an appropriate scale to the space with fixed parapet height and the proximity of development of a domestic scale at the edges of the site lead to the only conclusion that if a tall building is to go anywhere it can only be accommodated on the site of Block I2.’

8.17 At the time of the Outline consent concerns were expressed about the impact that a building that extended to the maximum parameter plan envelope could have on the site context. For this reason the maximum floor area of the block was set at 75% of the block as a whole. The Design and Access Statement set out how such a volume may be brought forward for example by providing two linked blocks with a full height atrium.

- 8.18 The architects for 50 and 60 Station Road have revisited the Design and Access Statement. In particular they have considered massing, rhythm, height and articulation. The pair of buildings that is proposed reflects the two linked buildings massing option that was set out the Design and Access Statement. The rhythm of building volumes and open spaces along Station Road is also respected and the pair of buildings is set forward in the street that was another key requirement of the original Design and Access Statement.
- 8.19 60 Station Road is one storey higher than 50 Station Road that supports the principle established by the Masterplan that building height should increase along Station Road and culminate in this location. The assumption that Block I2 would only be built out to 75% of its potential development envelope also means that the building needs to be strongly articulated to reduce its mass. The proposed buildings have achieved this and I explain this in more detail below.
- 8.20 The applicants have clearly considered the key elements of the original Design and Access Statement for this part of the Station Area Development. In general the principles of the Design and Access Statement have been respected. The main difference between the approved Parameter Plans and the development that is being brought forward is the extent of the footprint of the block.

Footprint of the building in comparison with approved Masterplan

- 8.21 A plan has been provided which shows the maximum approved footprint of Blocks I1 and I2 and the approved alignment of the SAR overlaid on the current scheme. This plan shows that the western edge of the SAR is to be relocated between 11 m and 19 m to the east. The eastern elevation of the new building is positioned between 11 m and 16 m beyond the eastern edge of Block I2 as approved. On the Station Road frontage the proposed buildings are 11 m or 20% wider than the approved Block I2 and to the rear 16 m or 30% wider. This arrangement has not changed in comparison with the previous scheme but no. 50 has been set back by three metres in response to concerns about the scale and massing of the buildings on Station Road. The effect of this is explored in detail below.

Building Design – Layout

- 8.22 The pair of buildings presents a frontage to Station Road 56 m wide. From second floor level upwards there is a 9 m wide gap between the two buildings and the floor plans for each block are rectangular. No. 50 is set back 3 metres further from Station Road than no. 60 where previously the buildings were in the same alignment. Both buildings incorporate a colonnade at ground and first floor. On no 60 the upper floors of the building project over the colonnade to 6 metres from the edge of the carriageway on Station Road. For no. 50 this distance is 9 metres from the carriageway.
- 8.23 Within the colonnade to no. 60 there is a double height glazed projection that serves as a reception area. A similar glazed area is provided to no. 50. Between the two buildings at first floor level is a glazed 'pod', which is accessible from both buildings and could serve as a meeting space. The stair core serving 60 Station Road projects from the Northeast corner of the building at the Station Road/SAR junction.
- 8.24 Retail space is accommodated in the Southeast corner of 60 Station Road. It is also set back under a colonnade by approximately 2.8 m facing the SAR and between 2 m and 5.8 m to the rear facing the public square. The south eastern corner of the building is chamfered under this double height colonnade and the southern elevation at ground and first floor level is set at an angle. An access road wraps around the rear of the building serving the basement car park, cycle store and servicing area.
- 8.25 The layout of the building at ground and first floor level is highly articulated i.e. it is not a simple 'boxlike' structure. The double height colonnade and the 'cutting back' of the south elevation will help to give the building a 'human scale' on the street and significantly reduce its mass and bulk at the lower levels. However, the earlier scheme, which also incorporated these features, was refused because of concerns about the scale and mass of the building at all levels. In my view the revision, which sets no. 50 back, though subtle, does have a significant effect on the way no. 50 will read in the street.

- 8.26 The previous scheme for 50 and 60 addressed the aims of the Masterplan in terms of the building being the tallest new building in the Masterplan area and being set forward in the street to close down and focus the views along Station Road on the station buildings. In making changes to the layout of the pair of buildings it is important in my view not to deviate from these aims, which were clearly understood and accepted as the Outline planning stage. I have addressed the issue of building height below. In terms of layout the setting back of no. 50 still facilitates the closing down of the view. It also allows for a more subtle stepping back of buildings on the south side of Station Road where no. 60 is the furthest forward and the J blocks furthest back with no. 50 adopting a mediating role between the blocks.

Building Design – Height

- 8.27 The overall height of the building is not constrained by the Outline Planning Consent in this case. However it is useful to compare the proposed scheme with the approved Parameter Plans. This will enable a consideration to be made about how well the building will sit in the overall Masterplan.

Table – Comparison between approved Parameter Plans and Proposed Development		
	60 Station Road	50 Station Road
<u>Parameter Plan</u> <u>Height</u> occupied floorspace	34.1m	34.1m
<u>Parameter Plan</u> <u>Height</u> including Plant/Lift Overrun	36.1m	36.1m
Proposed Height occupied floorspace	31.4m	28 m
Proposed Height including Plant/Lift Overrun	35.2m	32m

Proposed Height to Parapet	32m	28.8m
Proposed Height to Stair Tower	34.8m	n/a
Proposed Top Floor Set Back North elevation/Station Road 9 (excluding stair tower)	4.6m	4.6m
Proposed Top Floor Set Back South elevation/Public Square (excluding stair core)	4.6m	4.6m

- 8.28 The table demonstrates that both buildings sit within the parameters for the maximum height of Block I2. 60 Station Road is proposed to be one storey taller than 50 Station Road at 9 storeys plus roof plant. I share the earlier views of the Urban Design and Conservation Team that this is the correct approach and is an advantage over the earlier (2011) scheme for the site. 60 needs to be a more dominant structure to meet the vision of the Masterplan which is to have increasing building heights along the south side of Station Road culminating at this point.
- 8.29 The stair tower on the north eastern corner of number 60 further emphasises the visual importance of this building and provides a highly appropriate visual end stop for the access on the opposite side of Station Road. Number 50 is subservient in terms of height and this combined with the absence of any projecting stair tower and setting back by 3 metres reduces the visual impact of number 50 in relation to number 60 on Station Road.
- 8.30 The application as submitted proposed a reduction in parapet height to no. 50 to address the reason for refusal in terms of the adverse impact of scale and mass to Station Road. This involved the omission of the columns that projected above the

main body of the building and reduced the parapet height to 26.2 m a reduction of 2 metres. However officers did not support this revision because it revealed a view of the upper floor and plant enclosure, which was undesirable and inconsistent with no. 60. The plans have been adjusted to return to the earlier parapet height. In my view this is the correct approach because this reduction in height would not have had a significant effect on the scale and massing of the building on Station Road and introduced some unfortunate consequences for the streetscene.

- 8.31 To the south the height differential also works well. The taller number 60 will sit opposite the tallest block in the Blue Phase at 22.5m forming the south and north sides of a new public space. Number 50, at its lower height will sit more comfortably with the existing block on Warren Close.
- 8.32 I have no objections to the approach that has been adopted to building height. Both buildings will sit well with other building in this part of the Masterplan.

Building Design – Elevations and Materials

- 8.33 The treatment of the elevations and the material are very similar for the two buildings. The predominant material is reconstituted stone which is used to set up a grid which wraps around both buildings. Glazing is set back behind the stone grid and at roof level where the building is set back zinc panels and glazing is used.

Station Road

- 8.34 The Station Road elevation is crucial in marking the arrival point for the building and addressing what is the most important street in the Masterplan Area. The stair tower to number 60 is a deliberately dominant feature and will have a significant impact on the streetscene. It will be finished in natural stone and glazing which will allow views into the internal staircase. The stair tower projects from the front of the building on a triangular footprint and will be visible to users of Station Road traveling east or west. It will provide a good visual termination for the linear open space on the south side of Station Road and the access road opposite.

- 8.35 The colonnade allows the buildings to be set back from the street at ground and first floor level but the solidity is maintained internally so that the buildings 'meet the street' and an appropriate base is provided. The Addendum to the Design and Access Statement explains how the solid element behind the reception area would work.
- 8.36 The first floor pod that projects out from between the two buildings sits on a concrete slab and is fully glazed; it has a 'green' roof. The glazed upper floors behind the stone grid provide an appropriate 'middle' section for the building and are reminiscent of the Deity buildings to the west. The double height grid on the upper floors below parapet level invigorates the facade and helps to reduce the mass and bulk to the buildings.

Elevations to Public Space/SAR/Warren Close Access Road

- 8.37 These elevations of the buildings are more subtle than the Station Road elevation. 60 Station Road retains its two storey colonnade to the south but 50 Station Road does not. This approach is appropriate because the cycle store is located to the south of 50 Station Road. The gap between the two buildings is maintained above ground floor level and access is provided to the ramp serving the underground car park and the cycle parking area at ground/semi basement level.
- 8.38 The elevation to the SAR includes the projecting stair tower, which is glazed to allow full view of the stairs and the colonnade continues along this elevation. The principle of the stone grid is followed but the vertical spacing becomes tighter at the northern end to reflect the internal arrangements of the building. The same method is employed on the west elevation of 50 Station Road where it faces the Warren Close access road to accommodate the stair core, which is a much less dominant feature on this building. There is no colonnade on the western side where the building sits flush with the pavement. This elevation has been amended to address the comments raised by the UDC team and office accommodation now fronts Warren Close.
- 8.39 In my view the treatment of the elevations and the choice of materials are very successful. The elevational treatment will reduce the scale of what was always to be a significant building

in the Masterplan and the materials reflect the need to produce buildings of high quality as part of the redevelopment of this area.

Building Design – Phasing

- 8.40 The pair of buildings has been designed so that they can either be built together or as two separate phases. The applicant explains in the Design and Access Statement that this assists in the viability of the development. It is likely that Number 60 would come forward first because it occupies a site that is currently vacant. The Design and Access Statement illustrates what the streetscene would be like if only Number 60 is built. Although there is a great disparity between the proposed height of Number 60 and the existing height of 32-38 Station Road this is not uncommon in this area of Major Change. I do not think that there is any justification for requiring that the development be carried out as one project or that this can be controlled.

External Space – Realignment of the SAR

- 8.41 The realignment of the SAR is not significant in itself but it does introduce changes to the external environment around the proposed buildings and the wider Masterplan. The SAR previously had an alignment that formed part of a wider grid of routes through the site to the rear of the blocks fronting the Station Square and the Bus Interchange. The function of this route is unchanged by the realignment but the visual impact is altered. Instead of forming a crossroad with the access running to the North side of Station Road the SAR is off set and the stair tower to Number 60 closes the vista. The grid of routes through the area was considered to be of importance to the Masterplan but I do not think it was given such a degree of importance that it is sacrosanct. The realignment of the SAR is crucial to the delivery of the development on this site and in my view should be supported.

External Space – Design and Materials

- 8.42 The development delivers the SAR and completes another part of the public square to the south. The SAR will be surfaced in asphalt with red granite setts to mark the junction and the entry into the public square. This treatment continues into the servicing space and entry to the car park/cycle park. The public

square and the hard surfaced areas around the building will be finished in paving slabs to match the hard surfacing elsewhere on CB1.

8.43 There are six types of planted area in and around the building:

Tree planting to the Station Road and SAR frontages (see below)

A Green Roof on the projecting pod to the Station Road frontage

Sedum roofs on the cycle shelter

Liquidambar trees on the wall to the service area

An elevated terrace garden over the Car Park ramp to the south.

A roof terrace on the top floor of each building

The details of these planting areas are set out in the Landscape Proposals document. Both the terrace areas are accessible to people occupying the buildings.

On street cycle parking is accommodated on the Station Road and SAR frontage. The layout of this cycle parking differs from the previous scheme and provides for a crossing point on the SAR.

The Landscape Officer raises concerns about the use of trailing plants on wires above the car park ramp. This was a feature of the earlier scheme and no objections were raised at that time. This space is only visible from within the offices themselves and whilst I think trailing plants would be advantageous they would not be visible from the public realm. I am of the view that this is an issue that could be left to the landscape architect for the scheme.

External Space – Tree removal and planting

8.44 There are existing trees in front of and behind 32-38 Station Road, which will all be removed as part of the development. Agreement in principle to the removal of these trees was given as part of the Outline Planning permission.

8.45 New trees are to be planted as part of the development in the form of five small leaved lime trees on the Station Road frontage and four pear trees on the SAR. The lime trees have an

ultimate height and spread of 10 m by 4.5 m and the pear trees 8 m by 3 m.

Nature Conservation

- 8.46 The Ecology report that supports the application refers to the wider scheme for ecological mitigation that has already been agreed in relation to the wider development. The contribution that this site is to make is in terms of tree and shrub planting, features such as kestrel boxes and bat tubes are proposed on adjacent buildings. The additional information that has been submitted clarifies this point and both the City Council's Nature Conservation Officer and Natural England support this approach.

Conclusion - Context of site, design and external spaces

- 8.47 Officers have worked hard with the architects and landscape consultants to address the concerns raised in relation to the earlier applications. In my view this work has been fruitful and the revised plans show that the building will be of a very high quality, which is crucial for this part of the Masterplan. The revised scheme has been presented to the CB1 Sub-Panel of the Design and Conservation Panel. Members of the Sub-Panel are highly supportive of the building design and are of the view that the subtle changes that have been made deliver a significantly enhanced scheme.
- 8.48 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 3/4, 3/7, 3/11, 3/12.

Impact on Heritage Assets

- 8.49 The application is supported by a Heritage Statement as required by paragraph 128 of the National Planning Policy Framework (NPPF). Officers in the Urban Design and Conservation Team have not raised any concerns about this analysis and support the scheme subject to the imposition of planning conditions to address matters of detail.
- 8.50 The applicants have correctly identified the heritage assets that are affected by the development as 32-38 Station Road which are Buildings of Local Interest (BLIs) and the Conservation Area including the Station and associated buildings, the villas on the

north side of Station Road and the Mill. The NPPF includes buildings that are locally listed in the definition of a heritage asset.

- 8.51 The significance of 32-38 Station Road has been assessed using the NPPF and English Heritage's document Conservation Principles, Policies and Guidance 2008. The aspects of moderate significance have been identified as the evidential value as example of a late Victorian terrace that has been the subject of removal of some features of interest and the aesthetic value of the front elevation.
- 8.52 Aspects of low significance included the historical value, communal value, the south/east/west elevations, the interior and the setting. No elements of high significance were recognised.
- 8.53 The level of significance of this part of the Conservation Area is considered by the applicant to be moderate. This is because 32-38 Station makes a moderate contribution to the character.
- 8.54 The demolition of 32-38 Station Road is addressed in detail in my report about the application for Conservation Area Consent (CAC). That report concludes that the CAC should be supported provided that the replacement buildings are an appropriate replacement that will enhance the Conservation Area.
- 8.55 The Senior Conservation Officer's view on the CAC is as follows:
- ‘The loss of BLIs in a prominent location in the CA is always a matter for regret and the policies at national & local level are against such a loss except in cases where there is a demonstrable benefit to be derived. In this case, the decision makers who granted outline permission for the ‘masterplan’ were aware of the presumed demolition and assessed it to be worthwhile. All the discussions about the replacement building were predicated on the design having to be assessed as of suitable quality to meet the policy ‘tests’.’
- 8.56 I am convinced that the new office buildings will be a positive asset to the Conservation Area. The Design and Conservation

Panel CB1 Sub Panel, English Heritage and the Urban Design and Conservation Team share this view. Of particular note are the views of the Sub Panel, which are that the removal of the heritage asset is justified, as it would enable the implementation of the CB1 Masterplan as a whole, and enhance the Conservation Area.

- 8.57 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 4/11 and 4/12 and guidance provided by the NPPF.

Public Art

- 8.58 The application does not bring forward any detailed proposals for public art. It is anticipated that this will be secured via the s106 Agreement that will require the submission and approval of a Public Art Delivery Plan. In my view this is an appropriate way forward. I would normally expect a development of this scale to include public art proposals within the planning application, however in this case I do not think this is essential.
- 8.59 Although the application is a 'freestanding' full planning application in my view public art must be considered in the wider CB1 Masterplan context. The applicants are happy with this approach. A CB1 Public Art Strategy has already been agreed and pre-submission discussions have commenced on a proposal for public art in Station Road. It seems sensible to me that this site should be part of that proposal and this can be secured via Public Art Delivery Plan.
- 8.60 Subject to the submission and approval of a Public Art Delivery Plan, in my opinion the proposal is compliant with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and 9/8 and Cambridge Local Plan (2006) policies 3/7 and 10/1 and the Public Art SPD 2010

Renewable energy and sustainability

- 8.61 The application is supported by an Energy and Sustainability Statement and BREEAM Pre-Assessment. A Sustainable Development Checklist has also been completed. Both the Senior Sustainability Officer and the Sustainable Drainage Officer have been working closely with the applicant's consultants.

8.62 The Energy and Sustainability Statement indicates that the emphasis of their approach has been through passive design such as improved building fabric and external shading. Photovoltaic and solar thermal panels are also incorporated in the revised design. The Senior Sustainability Officer is satisfied with this approach and accepts that the development is not strictly compliant with Policy 8/16. The approach to meeting BREEAM 'excellent' and the overall levels of carbon reduction being achieved are fully supported.

8.63 In my opinion the applicants have suitably addressed the issue of sustainability and renewable energy and the proposal is in accordance with the Sustainable Design and Construction SPD 2007.

Disabled access

8.64 The Design and Access Statement does not address the question of disabled access in any great depth. However the Access Officer has no objections. He has raised a few issues that can be dealt with by conditions/informatives.

8.65 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 3/7 and 3/12.

Residential Amenity

Impact on amenity of neighbouring occupiers

Warren Close

8.66 The closest residential units are the flats on Warren Close development to the south of the site of 50 Station Road. A single block accommodates numbers 130 to 153 Warren Close. This block sits approximately 9 metres off the site boundary at its closest point and will be 21 metres from the main body of the office building. Car parking which serves the flats sits adjacent to the boundary. The flat block is 6 storeys high and therefore not an insubstantial building in itself but 50 Station Road will be two storeys higher plus roof plant. The principle outlook from the flats is toward the open space to the south but there are some secondary windows and the stair core on the north elevation facing the application site.

- 8.67 The new buildings will sit to the north of the existing flat block, which means the flats will overshadow the offices and not the other way around. The key residential impacts therefore arise from overlooking, increased sense of enclosure, loss of privacy and noise and disturbance.

Overlooking and loss of privacy

- 8.68 There is potential for overlooking or interlooking between the offices and the flats but the impact of this is reduced significantly by the size and secondary nature of window on the north side of the flats. The only internal space that will be overlooked is the stair core and externally the car park. I do not consider that this will be an adverse impact and could be argued as a benefit in terms of natural surveillance.

Increased sense of enclosure

- 8.69 The relative scale of the buildings will lead to an increased feeling of enclosure particularly in the car park area serving the flats. However this is more than compensated for by the larger public space, which will be provided to the northeast.

Noise and disturbance

- 8.70 The location of the access to the car park and the cycle park may generate additional noise to the north of the flats but in my view the level of disturbance unlikely to be significant. The Environmental Health Officer (EHO) has recommended conditions relating to construction activities, opening times, plant noise and odour that I have included in my recommendation.
- 8.71 The other flats within Warren Close will be further away from the development and will not be significantly affected once the buildings are completed. It is worth noting that no representations have been received from residents of Warren Close.

CB1 Blue Phase

- 8.72 This phase of the CB1 development is currently under construction to the south of the application site. Block L1 is the

closest to the site. It will be located 15 metres from the site boundary and there will be a minimum building-to-building distance of 30 metres. The space between Block L1 and 60 Station Road will form the new public space between the Park and the Station Square.

- 8.73 Block L1 is a substantial building at 7 storeys but 60 Station Road will be two storeys higher plus roof plant. The orientation is favourable in terms of overshadowing and the impacts on residential amenity will be similar to those described in relation to the Warren Close flats. The key difference is that some flats in Block L1 have a principal outlook toward the new offices; however the potential overlooking impact is mitigated to some degree by the separation distance of 30 metres.
- 8.74 In my opinion the proposal adequately respects the residential amenity of its neighbours and the constraints of the site and I consider that it is compliant with Cambridge Local Plan (2006) policies 3/4 and 3/7.

Refuse Arrangements

- 8.75 Space for storage of waste is provided in the basement and a collection point is identified adjacent to the service bay. The EHO is content with this arrangement subject to a condition to secure the detailed arrangements. The Waste Management Strategy that has been submitted references the RECAP Waste Management Design Guide.
- 8.76 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policy 3/12 and conforms to the RECAP Waste Management Design Guide.

Other environmental impacts

- 8.77 The EHO has identified the following issues as of relevance to the consideration of the application. I have set out below my recommendations on how they can be addressed:

Construction Phase Impacts – these can be addressed by the imposition of a condition to require the submission and approval of a Demolition and Construction Environmental Management Plan (DCEMP). I can see no reason why this should not be

related to the Construction Environmental Management Plan that has been agreed for the wider site.

Road traffic noise – I have recommended a condition that will enable suitable glazing to be installed to address this issue.

Plant noise – I have recommended a condition to secure details of plant to protect the amenity of neighbours.

Odour – the occupier of the retail/café area is not yet known and I have recommended a condition to secure odour controls should this prove necessary.

Opening Times – I have recommended a condition that restricts opening times and deliveries to the retail/café/office uses.

Air Quality – the EHO is satisfied that the level of car parking is such that it will not have an adverse impact on air quality.

Contaminated land – the eastern part of the site (60 Station Road) has been adequately assessed but a condition is required to address the western part (50 Station Road).

- 8.78 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 4/13 and 4/14.

Transport Impact

- 8.79 The application is supported by a Transport Assessment (TA) that has been carried out under the guidance of the County Council. The TA addresses the existing conditions, the proposed development, trip generation and assignment and junction capacity. It concludes that the impact of additional traffic will have a minimal effect on the operation of the Station Road/Tenison Road and Hills Road/Station Road junctions. It also asserts that the proposed traffic is likely to replace outgoing traffic from the existing development and that there will be a reduction in traffic on Station Road arising from fewer bus movements.
- 8.80 The County Council have reviewed the TA and have not raised any concerns about the conclusions that have been reached.

- 8.81 The s106 Agreement associated with the Outline planning consent required improvements to the Hills Road/Station Road junction prior to the commencement of occupation of the Red Phase of the development. The application site is within the Red Phase but its occupation will not automatically trigger the junction improvements because it would be a freestanding permission. Given that this trigger was regarded as an appropriate timescale for the impact of the CB1 development to begin to affect the use of the junction it is my view that it would be reasonable to link the occupation of 50 or 60 Station Road to the completion of the improvement works via the s106 Agreement.
- 8.82 In a similar way the s106 Agreement associated with the Outline planning consent allowed commuted payments towards SCATP and the CGB to be deferred from the first/Yellow Phase to the Red Phase. This was to reduce the burden of commuted payments in addition to payments to Network Rail for improvements to the Station. Although the development of 50 and 60 would not automatically trigger the payment of these deferred sums it is appropriate to secure them via the s106 Agreement on the basis that the trigger point of the commencement of the Red Phase as effectively been reached. The applicant is happy with this approach.
- 8.83 The applicants have provided a detailed study of the floorspace proposed against floorspace permitted and have taken the deferred payments into account. This results in a contribution towards SCATP of £213,745 and CGB of £758,630. I have set out below a comparison between this figure and that required via the Outline s106 for Block I2 only in my section on Planning Obligations.
- 8.84 In my view the works to the junction and the commuted payments towards SCATP and CGB adequately address the transport impacts of the development. In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 8/2, 9/9 and 10/1.

Highway Safety

- 8.85 The application includes the realignment of the Southern Access Road and the detailed access arrangements for the building. The Highway Authority has raised no objection to

these details on the grounds of highway safety. However the volume of traffic generated by the development, in conjunction with anticipated additional traffic from the wider development triggers the need for improvements to be made to the Hills Road/Station Road junction. These improvements will need to be secured via the s106 Agreement.

- 8.86 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policy 8/2.

Car and Cycle Parking

Car Parking

- 8.87 A total of 76 car parking spaces are provided in the basement including six spaces that are large enough for use by disabled people. The adopted car parking standards allow for a maximum of one car parking space per 100 m² of office floorspace and disabled parking only for retail uses. By application of these standards a maximum of 164 car parking spaces could be provided. The level of car parking provision is lower than this maximum and equates to 1 space per 209 sqm. However the level of car parking has been increased in comparison with the previous scheme.
- 8.88 The following table sets out the comparative numbers and ratio of car parking in the CB1 development to date.

SITE ADDRESS	NUMBER OF SPACES	CAR PARKING RATIO
50/60 Station Road (first scheme)	60	1 space per 274 sqm
50/60 Station Road (current scheme)	76	1 space per 209 sqm
Microsoft (excluding temporary car park)	35	1 space per 280 sqm
Block J2	40	1 space per 186 sqm

8.89 The level of car parking provision is comparable with other approved development on CB1. In my view this is appropriate given the highly accessible nature of the site by non-car modes and the low level of air quality. However lack of car parking was a reason for refusal of the previous scheme as follows:

The development fails to make adequate provision for car parking which would be likely to result in overspill parking into nearby residential areas, which would have a detrimental impact on the amenity of residents of those areas. The development is therefore contrary to policy 8/10 of the Cambridge Local Plan 2006.

8.90 This reason for refusal reflected the concerns of the Committee about the potential for overspill car parking to have an adverse impact on residential amenity in those areas where there are no controls on street. This concern has in part been addressed through an increase in provision of car parking to serve the development as described above. Also the recommendation of the Highway Authority which secures a pre- and post-development parking survey and funding for a Residents Parking Scheme if necessary serves to mitigate such potential adverse impact.

8.91 In my view it would be difficult to justify refusal of planning permission on the basis of lack of on site car parking provision.

Cycle Parking

8.92 The application has been revised so that a total of 572 cycle parking spaces are located in and around the building. Application of the adopted cycle parking standards indicates that up to 27 spaces should be provided for use by the retail units and 243 spaces for 50 Station Road and 287 spaces for 60 Station Road a total of 557 cycle spaces. The cycle parking provision accords with planning policy in terms of overall numbers. I have recommended a condition that addresses the issue of phasing of the development.

8.93 The cycle parking as revised delivers cycle parking in four ways:

- Ground level cycle parking on Sheffield stands between the office buildings (60 spaces)

- ☐ Internal Cycle Store within 50 Station Road -Sheffield stands 74 spaces and Double Stackers 82 spaces. (156 spaces)
- ☐ External Cycle Store rear of 50 Station Road – Sheffield stands 8 spaces and Double Stackers 206 spaces (214 spaces)
- ☐ 142 spaces on Sheffield stands adjacent to Station Road, the Southern Access Road and to rear of 60 Station Road.

8.94 I support the concept of a mix of types of cycle parking (50% double stackers/50% Sheffield stands).

8.95 25% of cycle parking is on street. This has a visual impact that I have addressed above and is challenging in terms of availability for use by the occupiers of the development and their visitors. I have raised this issue with the applicants and their response is that the CB1 'estate' will be a managed environment and they consider the occupiers of the building and the Management Company will be able to control the use of cycle parking spaces. They have submitted a Cycle Parking Management Plan as part of the Travel Plan that details how this will work.

8.96 The Cycle Parking Management Plan includes a cycle parking management strategy, which will consist of the following:

- ☐ Discreet signage on the stands to deter authorised use
- ☐ Allocation of a space on arrival for visitors
- ☐ Active surveillance of cycle parking
- ☐ Registration of cycles used by staff and the issue of a Bicycle Permit to be displayed on the cycle
- ☐ Requests for immediate removal of unauthorised cycle by concierge
- ☐ Removal of unauthorised cycles within 24 hours of two written warnings.

8.97 A similar system currently operates at the Mott MacDonald Offices on Station Road. In my view this level of control is acceptable and will ensure that cycle parking space is available for authorised users only. I have recommended a condition to

secure submission and approval of the Travel Plan of which the Cycle Parking Management Plan forms part.

8.98 The details of the Travel Plan and its implementation need to be secured by the s106 Agreement and secured by condition.

8.99 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 8/6 and 8/10.

Third Party Representations

8.100 I have addressed the majority of the issues raised via third party representations above. The following table indicates the relevant sections of my Assessment.

Issue	Reference
Previous proposals and current applications	Context of site, design and external spaces
Loss of Existing Buildings	Impact on Heritage Assets/Report for Conservation Area Consent
The New Buildings	Context of site, design and external spaces
Other Issues relating to Trees, Amenity and Parking	Context of site, design and external spaces Residential amenity Car and cycle parking

8.101 Third party representations also raise concerns about procedural issues and the s106 Agreement.

Procedural Issues

A separate full application should not have been submitted rather than a Reserved Matters application. If the full application were to be agreed it would create an unwelcome precedent,

and might render the original scheme, agreed at outline stage, redundant.

The Council has no powers to require that a submission of reserved matters be made. The Outline consent will continue to be a very significant material consideration for future development in the area.

An Environmental Impact Assessment should be required. The application is not a Reserved Matters application: it is a full stand-alone application and must be viewed entirely on its own merits and not with reference to the CB1 Masterplan agreed in outline.

The applicants have requested a Screening Opinion under the Environmental Impact Regulations to establish whether an Environmental Impact Assessment (EIA) is needed. This is supported by a Legal Opinion by a leading Counsel in this field.

A full Traffic Impact Assessment is also required, as is consideration of other matters such as car parking, open space provision, accessibility to emergency vehicles and waste collection and demolition of existing buildings.

The application is supported by a TA and other relevant information.

Planning Obligations

Community Infrastructure Levy Regulations 2010

8.102 The Community Infrastructure Levy Regulations 2010 have introduced the requirement for all local authorities to make an assessment of any planning obligation in relation to three tests. If the planning obligation does not pass the tests then it is unlawful. The tests are that the planning obligation must be:

- (a) necessary to make the development acceptable in planning terms;
- (b) directly related to the development; and
- (c) fairly and reasonably related in scale and kind to the development.

8.103 In bringing forward my recommendations in relation to the Planning Obligation for this development I have considered these requirements. The Planning Obligation Strategy (2010) provides a framework for expenditure of financial contributions collected through planning obligations. The Affordable Housing Supplementary Planning Document 2008 provides guidance in terms of the provision of affordable housing and the Public Art Supplementary Planning Document 2010 addresses requirements in relation to public art (amend/delete as applicable). The applicants have indicated their willingness to enter into a S106 planning obligation in accordance with the requirements of the Strategy and relevant Supplementary Planning Documents. The proposed development triggers the requirement for the following community infrastructure:

S106 Agreement relating to the Outline Planning Consent

8.104 Although this is a full planning application, which would result in the grant of a freestanding planning permission, it is my view that an understanding of the s106 Agreement for the Outline consent remains important. 50/60 Station Road will be constructed on the site of what would have been Block I2 and part of Block I1. Block I2 falls within the Red phase of the development and Block I1 in the Green phase.

8.105 The implementation of development in the Red Phase e.g. Block I2 would trigger the following commuted payments/infrastructure under the Outline consent:

- ☐ Submission and approval of the Public Art Delivery Plan for the Red Phase.
- ☐ Local User condition restriction
- ☐ Relocation Strategy for Woodlands Surgery
- ☐ Hills Road/Station Road junction works
- ☐ Sub-phase payments towards SCATP and CGB including deferred payments
- ☐ Agreement of Travel Plan Co-ordinator and Travel Plan

8.106 The implementation of development in the Green Phase e.g. Block I1 would trigger the following commuted payments/infrastructure under the Outline consent:

- ☐ Submission and approval of the Public Art Delivery Plan for the Green Phase

- ☐ Local User condition restriction
- ☐ Scheme for Station Square including a Management Plan
- ☐ Sub-phase payments towards SCATP and CGB
- ☐ Submission of details of the Northern Access Road
- ☐ Agreement of Travel Plan Co-ordinator and Travel Plan

8.107 The mitigation measures that are identified for the Red Phase are capable of being secured via the section 106 Agreement for 50/60 Station Road for the reasons that I have set out in my Assessment. Only a small part of the Green Phase falls within the application site and development of this site does not preclude development within Block I1. Under these circumstances I think it would be unreasonable to expect this development to comply with the requirements for the Green Phase. Applications have been submitted for other parts of the Green Phase, which are under consideration.

Transport

8.108 Contributions towards the SCATP and CGB have been brought forward by the applicants as part of the application. The s106 for the Outline Planning consent required the payment towards SCATP and CGB on commencement of Block I2, which is identified as Part C of the Red Phase for the purposes of the s106 Agreement only.

8.109 The following table sets out the commuted payments that are due under the Outline Planning Permission in all three sub phases of the Red Phase. These figures include payments of £261,093 for SCATP and £926,604 for CGB that were deferred from the Yellow Phase and the contributions that arise from the development of the Red Phase (SCATP £223,483 and CGB £793,126) a total of £2,204,306* for these transport impact mitigation measures.

Table – SCATP/CGB Contributions as set out in s106 for Outline Consent

Red Sub Phase	SCATP contribution	CGB contribution
Part A (J1/J2)	£184,119	£653,497

Part B (J3/J4)	£129,363	£459,168
Part C (I2)	£171,055	£607,066
Total	£484,537	£1,719,731
Overall Total		£2,204,268

* From Committee Report November 2009

**From s106 and subject to minor adjustment (£38)

8.110 The commuted sums that have been brought forward in connection with 50/60 Station Road have been increased to take account of the increased floorspace and the following payments will be made on commencement of each phase of the development:

Table - SCATP/CGB Contributions as offered by applicants in respect of 50/60 Station Road

Phase	SCATP contribution	CGB contribution
50 Station Road	£96,988	£344,233
60 Station Road	£116,757	£414,397
Total	£213,745	£758,630
Overall Total		£972,375
Total for Red Phase including Part A and Part B		£2,398,522

8.111 A comparison between the two tables demonstrates that the contributions that have been brought forward for 50/60 Station Road are compatible with those set out in the s106 for Block I2. The contributions include the deferred payments from Yellow Phase and take account of the increase in floorspace. It can also be demonstrated that if other parts of the Red Phase come forward as planned the total contribution toward transport mitigation in the form of commuted payments will be exceeded.

- 8.112 The development also generates the need for improvements to the Hills Road/Station Road junction when considered in conjunction with other development within the CB1 Masterplan area.
- 8.113 Overspill parking from the development has the potential to have an adverse impact on the amenity of residents in the vicinity of the site. A pre construction and post occupation parking survey is necessary to assess the impact of the development the outcome of which may be the establishment of a Residents Parking Scheme. The costs of carrying out the survey and setting up the Scheme should be borne by the applicant.
- 8.114 The funding and agreement of a Travel Plan Co-ordinator is also needed.
- 8.115 Subject to the completion of a S106 planning obligation to secure the commuted payments, junction improvements, car parking survey and mitigation and the Travel Plan Co-ordinator, I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1, P9/8 and P9/9, Cambridge Local Plan (2006) policies 8/2, 8/3, 9/9 and 10/1 and the Planning Obligation Strategy 2010.

Public Art

- 8.116 The development is required to make provision for public art and officers have recommended as set out in paragraphs 8.58 to 8.60 above that in this case provision for public art should be made on site via the submission and approval of a Public Art Delivery Plan (PADP). The PADP should be required to relate to the approved CB1 Public Art Strategy and the PADP for the Red Phase.
- 8.117 Subject to the completion of a S106 planning obligation to secure this infrastructure provision, I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and 9/8, Cambridge Local Plan (2006) policies 3/7, 9/9 and 10/1 and the Public Art SPD 2010.

Loss of Community Facility (Doctor's Surgery)

8.118A Relocation Strategy is needed to ensure that all reasonable endeavours are made to relocate the surgery before the development of 50 Station Road. I have addressed this issue in paragraphs 8.5 and 8.6.

8.119 Subject to the completion of a S106 planning obligation to secure the Relocation Strategy for Woodlands Surgery, I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and 9/8 and Cambridge Local Plan (2006) policies 5/11, 9/9 and 10/1

Occupation Restriction (Offices)

8.120A Local User Condition is needed to ensure that the development is occupied in accordance with Development Plan policy. I have addressed this issue in paragraph 8.11.

8.121 Subject to the completion of a S106 planning obligation to secure the restriction on occupation, I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and 9/8 and Cambridge Local Plan (2006) policies 7/2, 9/9 and 10/1

Monitoring

8.122 The Planning Obligation Strategy (2010) requires that all new residential developments contribute to the costs of monitoring the implementation of planning obligations. The costs are calculated according to the heads of terms in the agreement. The contribution sought will be calculated as £150 per financial head of term and £300 per non-financial head of term. Contributions are therefore required on that basis.

Planning Obligations Conclusion

8.123 It is my view that the planning obligation is necessary, directly related to the development and fairly and reasonably in scale and kind to the development and therefore the Planning Obligation passes the tests set by the Community Infrastructure Levy Regulations 2010. The planning obligation has a strong relationship with the s106 Agreement for the Outline Planning

Consent and will ensure that all the mitigation measures associated with Block I2 are secured for 50/60 Station Road.

9.0 CONCLUSION

- 9.1 This application is for a revised scheme, which seeks to overcome the reasons for refusal of the previous scheme, which is currently at Appeal. In my view, for the reasons set out in my report, the revised scheme is successful in this regard. Whilst not radically different from the previous scheme, the changes that have been made to the relationship of 50 Station Road to the road and the increase in car parking provision do have a significant impact.
- 9.2 I am not alone in my view that 50 and 60 Station Road are well-designed buildings that respond well to their context and will enhance the Conservation Area. The Design and Conservation CB1 Sub-Panel, English Heritage and the City Council's Design and Conservation team share my view.
- 9.3 The Committee are not bound by the constraints of the Outline consent but it is a very significant material consideration. The new buildings respect the approved Parameter Plans in terms of their height and broad location. The Screening Opinion for Environmental Impact Assessment concluded that the development does not amount to EIA development. The key to the assessment of this application and the related application for Conservation Area Consent for Wilton Terrace is to consider whether or not the new building is of a high quality appropriate for its setting and role as key building within the Station Area. In my view the quality of the building reflects its setting in an Area of Major Change and justifies the removal of Buildings of Local Interest in terms of being an appropriate replacement and bringing tangible public benefits in terms of the delivery of the wider Masterplan.

10.0 RECOMMENDATION

1. **APPROVE** subject to the satisfactory completion of the s106 agreement by 30 September 2013 and subject to the following conditions and reasons for approval:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In accordance with the requirements of section 51 of the Planning and Compulsory Purchase Act 2004.

2. All management and maintenance of landscaping shall be carried out in accordance with the approved Landscape Management Plan by Robert Myers Associates dated April 2012 PL2.

Reason: To ensure satisfactory arrangements are in place to ensure the proper management and maintenance of landscaped areas (Cambridge Local Plan 2006 policies 3/4, 3/7, 3/8, 3/11, 4/2, 4/3, 4/4 and 9/9).

3. All management and maintenance of ecology shall be carried out in accordance with the approved Ecology Report by RPS dated April 2012 as supplemented by Response to comments made on the Ecological Management Plan by RPS dated 5 July 2012.

Reason: To ensure satisfactory arrangements are in place to ensure the proper management and maintenance of ecology (Cambridge Local Plan 2006 policies 3/4, 3/7, 3/8, 3/11, 4/2, 4/3, 4/4 and 9/9).

4. Any trees or plants provided as part of any landscaping scheme, within a period of 5 years from the completion of the development, which die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of similar size and species as those originally planted, unless the local planning authority gives written consent to any variation. No development within the site for which reserved matters approval is sought shall commence until the landscaping scheme has been approved in writing by the local planning authority. The scheme shall be carried out in accordance with the approved details unless otherwise agreed in writing by the local planning authority.

Reason: To ensure satisfactory arrangements are in place for replacement planting to ensure proper provision of landscaped areas (Cambridge Local Plan policies 3/2, 3/4, 3/7, 3/11, 3/12, 4/2, 4/3, 4/4 and 9/9)

5. Prior to the commencement of works to provide the cycle store, full details of proposed facade greening/climbing plants to elevations of the proposed cycle stores adjacent to the southern boundary including a maintenance plan for its long term retention shall be submitted to and approved by the local planning authority in writing. The development shall be carried in accordance with the approved details.

Reason To ensure the satisfactory provision of green facades in the interests of long term visual amenity (Cambridge Local plan policies 3/7. 3/11 and 9/9)

6. Prior to the occupation of 50 Station Road, a certificate following a post-construction review shall be issued by an approved BREEAM Licensed Assessor to the Local Planning Authority, indicating that the BREEAM rating EXCELLENT or higher has been met. In the event that such a rating is replaced by a comparable national measure of sustainability for building design, the equivalent level of measure shall be applicable to the proposed development unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interests of reducing carbon dioxide emissions and promoting principles of sustainable construction and efficient use of buildings
(Cambridge Local Plan 2006 policy 8/16 and Supplementary Planning Document 'Sustainable Design & Construction' 2007).

7. Prior to the occupation of 60 Station Road, a certificate following a post-construction review shall be issued by an approved BREEAM Licensed Assessor to the Local Planning Authority, indicating that the BREEAM rating EXCELLENT or higher has been met. In the event that such a rating is replaced by a comparable national measure of sustainability for building design, the equivalent level of measure shall be applicable to the proposed development unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interests of reducing carbon dioxide emissions and promoting principles of sustainable construction and efficient use of buildings

8. The approved renewable energy technologies to meet the approved carbon emissions of 50 Station Road shall be fully installed and operational prior to the occupation of 50 Station Road and shall thereafter be maintained and remain fully operational in accordance with an approved maintenance programme, unless otherwise agreed in writing by the local planning authority.

Reason: In the interests of reducing carbon dioxide emissions (Cambridge Local Plan 2006 policy 8/16 and Supplementary Planning Document 'Sustainable Design & Construction' 2007).

9. The approved renewable energy technologies to meet the approved carbon emissions of 60 Station Road shall be fully installed and operational prior to the occupation of 60 Station Road and shall thereafter be maintained and remain fully operational in accordance with an approved maintenance programme, unless otherwise agreed in writing by the local planning authority.

Reason: In the interests of reducing carbon dioxide emissions (Cambridge Local Plan 2006 policy 8/16 and Supplementary Planning Document 'Sustainable Design & Construction' 2007).

10. 5% of all parking spaces shall be suitable for, and reserved for, people with disabilities.

Reason: To ensure an appropriate level of car parking provision for people with disabilities (Cambridge Local Plan policy 8/10 and appendix C).

11. Prior to commencement of development a delivery plan for the phased delivery of cycle parking for use in association with 50 and 60 Station Road shall be submitted to and approved by the local planning authority in writing. The approved facilities for each building shall be provided in accordance with the approved delivery plan before occupation of each building and shall thereafter be retained and shall not be used for any other purpose unless otherwise agreed in writing by the local planning authority.

Reason: To ensure appropriate provision for the secure storage of bicycles (Cambridge Local Plan 2006 policy 8/6).

12. Prior to the commencement of development, full details of access to below ground cycle storage areas shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details, which shall be fully implemented in accordance with the approved cycle parking delivery plan, and shall be retained thereafter unless otherwise agreed in writing by the local planning authority.

Reason: To provide convenient and safe access to cycle storage areas. (Cambridge Local Plan policy 8/6)

13. Prior to the commencement of first occupation full details of the security arrangements to provide for safe use of the basement car and cycle parking areas, shall be submitted to and approved by the local planning authority in writing. The approved provisions for safe use of car and cycle parking facilities shall be provided prior to the first occupation and shall be retained thereafter in respect of the occupation of both 50 and 60 Station Road unless otherwise agreed in writing by the local planning authority.

Reason: To provide convenient and safe access to cycle storage areas. (Cambridge Local Plan policy 8/6)

14. Prior to the commencement of development, with the exception of below ground works, a Travel Plan and Cycle Parking Management Plan shall be submitted to and approved by the local planning authority in writing. The approved Travel Plan and Cycle Parking Management shall thereafter be first implemented upon first occupation of either 50 or 60 Station Road and shall be maintained and implemented unless otherwise agreed in writing by the local planning authority.

Reason: To ensure satisfactory arrangements are in place to secure work place travel planning and the management of cycle parking.(Cambridge Local Plan 2006 policies 8/2, 8/3 and 8/6).

15. Prior to commencement of development, a detailed surface water strategy shall be submitted to and approved by the local planning authority in writing. The strategy shall demonstrate how the management of surface water within the context of the approved details of the CB1 strategic site wide surface water strategy. The strategy shall maximise the use of measures to control water where it falls as far as practicable to limit the rate (peak flow) and quantity (volume) of run-off and improve the quality of any run-off before it leaves the site and enters the strategic site wide system.

The strategy shall include details of all flow control systems and the design, location and capacity of all strategic SUDS features and shall include ownership, long-term adoption, management and maintenance scheme(s) and inspection arrangements/responsibilities, including detailed calculations, levels and flow routes to demonstrate the capacity of the measures to adequately manage surface water within the site without the risk of flooding to land or buildings.

The development shall be carried out in accordance with the approved surface water drainage strategy.

Reason - To ensure a satisfactory and sustainable method of surface water drainage and to prevent increased risk of flooding to third parties. (Cambridge Local Plan 2006 policies 4/13 and 8/18 and Supplementary Planning Document 'Sustainable Design & Construction' 2007).

16. Prior to commencement of development, a detailed foul water drainage strategy shall be submitted to and approved by the local planning authority in writing.

The development shall be carried out in accordance with the approved foul water drainage strategy.

Reason - To ensure a satisfactory and sustainable method of foul drainage and to prevent increased risk of flooding to third parties. (Cambridge Local Plan 2006 policies 4/13 and 8/18 and Supplementary Planning Document 'Sustainable Design & Construction' 2007).

17. Infiltration systems should only be used where it can be demonstrated that they will not pose a risk to groundwater quality. A scheme for surface water disposal needs to be submitted to and approved by the local planning authority. The scheme shall be implemented as approved.

Reason: The site is acknowledged to be potentially contaminated in submitted documents. Soakaways and other infiltration sustainable drainage systems (SUDS) must not be constructed in contaminated ground. The water environment is potentially vulnerable and there is an increased potential for pollution from inappropriately located and/or designed infiltration sustainable drainage systems (SUDS) such as soakaways, untanked porous pavement systems or infiltration basins. (Cambridge Local Plan 2006 policies 4/13 and 8/18 and Supplementary Planning Document 'Sustainable Design & Construction' 2007).

18. Piling or any other foundation designs using penetrative methods will not be permitted other than with the express written consent of the local planning authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater.

Reason. The site is potentially contaminated and intrusive foundation solutions could lead to the contamination of groundwater in the underlying aquifer. (Cambridge Local Plan 2006 policies 4/13 and 8/18 and Supplementary Planning Document 'Sustainable Design & Construction' 2007).

19. Prior to the commencement of any development, a scheme for the provision and implementation of pollution control of the water environment shall be submitted and agreed in writing with the Local Authority. The works/scheme shall be constructed and completed in accordance with the approved plans.

Reason. To prevent the increased risk of pollution to the water environment. (Cambridge Local Plan 2006 policies 4/13 and 8/18 and Supplementary Planning Document 'Sustainable Design & Construction' 2007).

20. Each phase of the development approved by this permission shall not be COMMENCED prior to a contaminated land assessment and associated remedial strategy, being submitted to the local planning authority and receipt of approval of the document/documents from the local planning authority in writing.

This applies to paragraphs a), b) and c). This is an iterative process and the results of each stage will help decide if the following stage is necessary.

(a) The contaminated land assessment shall include a desk study to be submitted to the local planning authority for approval. The desk study shall detail the history of the site uses and propose a site investigation strategy based on the relevant information discovered by the desk study. The strategy shall be approved by the local planning authority prior to investigations commencing on site.

(b) The site investigation, including relevant soil, soil gas, surface and groundwater sampling, shall be carried out by a suitable qualified and accredited consultant/contractor in accordance with a quality assured sampling and analysis methodology.

(c) A site investigation report detailing all investigative works and sampling on site, together with the results of the analysis, risk assessment to any receptors and a proposed remediation strategy shall be submitted to the local planning authority. The local planning authority shall approve such remedial works as required prior to any remediation commencing on site. The works shall be of such a nature as to render harmless the identified contamination given the proposed end use of the site and surrounding environment including any controlled waters.

Each phase of the development approved by this permission shall not be OCCUPIED prior to the completion of any remedial works and a validation report/s being submitted to the local planning authority and receipt of approval of the document/documents from the local planning authority in writing. This applies to paragraphs d), e) and f).

(d) Approved remediation works shall be carried out in full on site under a quality assurance scheme to demonstrate compliance with the proposed methodology and best practice guidance.

(e) If, during the works contamination is encountered which has not previously been identified then the additional contamination shall be fully assessed and an appropriate remediation scheme agreed with the local planning authority.

(f) Upon completion of the works, this condition shall not be discharged until a closure report has been submitted to and approved by the local planning authority. The closure report shall include details of the proposed remediation works and quality assurance certificates to show that the works have been carried out in full in accordance with the approved methodology. Details of any post-remedial sampling and analysis to show the site has reached the required clean-up criteria shall be included in the closure report together with the necessary documentation detailing what waste materials have been removed from site.

Reason: In order to ensure that any contamination is identified on the site before the commencement of the scheme, to enable any mitigation/remediation measures to be implemented during the development phase and to ensure protection of controlled waters. (Cambridge Local Plan policy 4/13)

21. Prior to the commencement of the development of 50 Station Road, including any demolition or enabling works, a Demolition and Construction Environmental Management Plan (DCEMP) shall be submitted to and approved in writing by the local planning authority. The DCEMP shall include the consideration of the following aspects of demolition and construction:

- a) Demolition, construction and phasing programme.
- b) Contractors' access arrangements for vehicles, plant and personnel including the location of construction traffic routes to, from and within the site, details of their signing, monitoring and enforcement measures.
- c) Construction/Demolition hours which shall be carried out between 0800 hours to 1800 hours Monday to Friday, and 0800 hours to 1300 hours on Saturday and at no time on Sundays, Bank or Public Holidays, unless in accordance with agreed emergency procedures for deviation. Prior notice and agreement procedures for works outside agreed limits and hours.
- d) Delivery times for construction purposes.
- e) Soil Management Strategy.
- f) Outline Waste Management Plan (OWMP).

- g) Noise method, monitoring and recording statements in accordance with the provisions of BS 5228-1: 2009.
- h) Maximum noise mitigation levels for construction equipment, plant and vehicles.
- i) Vibration method, monitoring and recording statements in accordance with the provisions of BS 5228-2: 2009.
- j) Maximum vibration levels.
- k) Dust management and wheel washing measures in accordance with the provisions of London Best Practice Guidance: The control of dust and emissions from construction and demolition.
- l) Use of concrete crushers.
- m) Prohibition of the burning of waste on site during demolition/construction.
- n) Site lighting.
- o) Drainage control measures including the use of settling tanks, oil interceptors and bunds.
- p) Screening and hoarding details.
- q) Access and protection arrangements around the site for pedestrians, cyclists and other road users.
- r) Procedures for interference with public highways, including permanent and temporary realignment, diversions and road closures.
- s) External safety and information signing and notices.
- u) Consideration of sensitive receptors.
- v) Prior notice and agreement procedures for works outside agreed limits.
- x) Complaints procedures, including complaints response procedures.
- y) Membership of the Considerate Contractors Scheme.

Thereafter the development shall be undertaken in accordance with the agreed details unless otherwise agreed in writing by the local planning authority.

Reason: To ensure the environmental impact of the construction of the development is adequately mitigated and in the interests of the amenity of nearby residents/occupiers (Cambridge Local Plan 2006 policy 4/13).

22. Prior to the commencement of the development of 60 Station Road, including any demolition or enabling works, a Demolition and Construction Environmental Management Plan (DCEMP) shall be submitted to and approved in writing by the local planning authority. The DCEMP shall include the consideration of the following aspects of demolition and construction:
- a) Demolition, construction and phasing programme.
 - b) Contractors' access arrangements for vehicles, plant and personnel including the location of construction traffic routes to, from and within the site, details of their signing, monitoring and enforcement measures.
 - c) Construction/Demolition hours which shall be carried out between 0800 hours to 1800 hours Monday to Friday, and 0800 hours to 1300 hours on Saturday and at no time on Sundays, Bank or Public Holidays, unless in accordance with agreed emergency procedures for deviation. Prior notice and agreement procedures for works outside agreed limits and hours.
 - d) Delivery times for construction purposes.
 - e) Soil Management Strategy.
 - f) Outline Waste Management Plan (OWMP).
 - g) Noise method, monitoring and recording statements in accordance with the provisions of BS 5228-1: 2009.
 - h) Maximum noise mitigation levels for construction equipment, plant and vehicles.
 - i) Vibration method, monitoring and recording statements in accordance with the provisions of BS 5228-2: 2009.
 - j) Maximum vibration levels.
 - k) Dust management and wheel washing measures in accordance with the provisions of London Best Practice Guidance: The control of dust and emissions from construction and demolition.
 - l) Use of concrete crushers.
 - m) Prohibition of the burning of waste on site during demolition/construction.
 - n) Site lighting.
 - o) Drainage control measures including the use of settling tanks, oil interceptors and bunds.
 - p) Screening and hoarding details.
 - q) Access and protection arrangements around the site for pedestrians, cyclists and other road users.

- r) Procedures for interference with public highways, including permanent and temporary realignment, diversions and road closures.
- s) External safety and information signing and notices.
- u) Consideration of sensitive receptors.
- v) Prior notice and agreement procedures for works outside agreed limits.
- x) Complaints procedures, including complaints response procedures.
- y) Membership of the Considerate Contractors Scheme.

Thereafter the development shall be undertaken in accordance with the agreed details unless otherwise agreed in writing by the local planning authority.

Reason: To ensure the environmental impact of the construction of the development is adequately mitigated and in the interests of the amenity of nearby residents/occupiers (Cambridge Local Plan 2006 policy 4/13).

23. Prior to occupation of 50 Station Road, full details of equipment for the purpose of extraction and/or filtration of fumes and/or odours shall be submitted to and approved in writing by the local planning authority. The scheme as approved shall be fully implemented prior to the occupation of the building.

Reason: To protect the amenity of nearby properties (Cambridge Local Plan 2006 policy 4/13)

24. Prior to occupation of 60 Station Road, full details of equipment for the purpose of extraction and/or filtration of fumes and/or odours shall be submitted to and approved in writing by the local planning authority. The scheme as approved shall be fully implemented prior to the occupation of the building.

Reason: To protect the amenity of nearby properties (Cambridge Local Plan 2006 policy 4/13)

25. Prior to occupation of 50 Station Road a noise attenuation/insulation scheme and/or phased attenuation measures (having regard to the building fabric, glazing and mechanical ventilation) shall be submitted to and approved by the Local Planning Authority in order to demonstrate the scheme shall achieve internal noise levels recommended in British Standard 8233:1999 'Sound Insulation and Noise Reduction for Buildings - Code of Practice'. The approved scheme shall be fully implemented and a completion report submitted prior to the occupation of 50 Station Road. The approved scheme shall remain unaltered in accordance with the approved details unless otherwise agreed in writing by the local planning authority.

Reason: To protect amenity of the occupants of noise sensitive development (Cambridge Local Plan 2006 policy 4/13).

26. Prior to occupation of 60 Station Road a noise attenuation/insulation scheme and/or phased attenuation measures (having regard to the building fabric, glazing and mechanical ventilation) shall be submitted to and approved by the Local Planning Authority in order to demonstrate the scheme shall achieve internal noise levels recommended in British Standard 8233:1999 'Sound Insulation and Noise Reduction for Buildings - Code of Practice'. The approved scheme shall be fully implemented and a completion report submitted prior to the occupation of 60 Station Road. The approved scheme shall remain unaltered in accordance with the approved details unless otherwise agreed in writing by the local planning authority.

Reason: To protect amenity of the occupants of noise sensitive development (Cambridge Local Plan 2006 policy 4/13).

27. Prior to occupation of 50 Station Road, full details of a scheme for the insulation of the building(s) and/or plant in order to minimise the level of noise emanating from 50 Station Road and/or plant shall be submitted to and approved by the local planning authority in writing. The scheme as approved shall be fully implemented prior to the occupation of 50 Station Road.

Reason: To protect the amenity of nearby properties (Cambridge Local Plan 2006 policy 4/13)

28. Prior to occupation of 60 Station Road, full details of a scheme for the insulation of the building(s) and/or plant in order to minimise the level of noise emanating from 60 Station Road and/or plant shall be submitted to and approved by the local planning authority in writing. The scheme as approved shall be fully implemented prior to the occupation of 50 Station Road.

Reason: To protect the amenity of nearby properties (Cambridge Local Plan 2006 policy 4/13)

29. Prior to the commencement of occupation of 50 Station Road, full details of the on-site storage facilities for trade waste, including waste for recycling shall be submitted to and approved in writing by the local planning authority. Such details shall identify the specific positions of where wheelie bins, paladins or any other means of storage will be stationed and the arrangements for the disposal of waste. The approved facilities shall be provided prior to the commencement of the use hereby permitted and shall be retained thereafter unless alternative arrangements are agreed in writing by the local planning authority.

Reason: To protect the amenities of nearby residents/occupiers and in the interests of visual amenity. (Cambridge Local Plan 2006 policy 4/13)

30. Prior to the commencement of occupation of 60 Station Road, full details of the on-site storage facilities for trade waste, including waste for recycling shall be submitted to and approved in writing by the local planning authority. Such details shall identify the specific positions of where wheelie bins, paladins or any other means of storage will be stationed and the arrangements for the disposal of waste. The approved facilities shall be provided prior to the commencement of the use hereby permitted and shall be retained thereafter unless alternative arrangements are agreed in writing by the local planning authority.

Reason: To protect the amenities of nearby residents/occupiers and in the interests of visual amenity. (Cambridge Local Plan 2006 policy 4/13)

31. Prior to the commencement of occupation of 50 Station Road, full details of the means by which waste will be collected from the site, including the means by which refuse containers will be moved to the street frontage for collection and returned to the refuse store after the collection of waste and the location of on-street storage on collection days, shall be submitted to and approved by the local planning authority in writing. The approved arrangements shall be retained thereafter unless alternative arrangements are agreed in writing by the local planning authority.

Reason: To protect the amenities of nearby residents/occupiers and in the interests of visual amenity. (Cambridge Local Plan 2006 policy 4/13)

32. Prior to the commencement of occupation of 60 Station Road, full details of the means by which waste will be collected from the site, including the means by which refuse containers will be moved to the street frontage for collection and returned to the refuse store after the collection of waste and the location of on-street storage on collection days, shall be submitted to and approved by the local planning authority in writing. The approved arrangements shall be retained thereafter unless alternative arrangements are agreed in writing by the local planning authority.

Reason: To protect the amenities of nearby residents/occupiers and in the interests of visual amenity. (Cambridge Local Plan 2006 policy 4/13)

33. The retail/café/restaurant units shall only be open for trade between 07:00 and 23:00.

Reason: To protect the amenity of nearby properties (Cambridge Local Plan 2006 policy 4/13)

34. Deliveries shall only be made to 50 and 60 Station Road and associated retail/café/restaurant units between 07:00 and 23:00.

Reason: To protect the amenity of nearby properties (Cambridge Local Plan 2006 policy 4/13)

35. No construction work shall be carried out or plant operated other than between the following hours: 0730 to 1800 Monday to Friday, 0800 to 1300 on Saturdays and at no time on Sundays, Bank or Public Holidays unless otherwise agreed in writing by the local planning authority in advance.

Reason: To protect the amenity of nearby residents/occupiers (Cambridge Local Plan 2006 policy 4/13).

36. Notwithstanding the information detailed on the approved plans, no building or structure shall exceed 50m AOD in height.

Reason: In the interests of safety and to safeguard the operation of Cambridge Airport. (Cambridge Local Plan 2006 policy 3/4)

37. Prior to the commencement of the development hereby approved, with the exception of below ground works, full details including samples of the materials to be used in the construction of the external surfaces of both 50 and 60 Station Road shall be submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 3/14)

38. Before starting any stone work (artificial and/or real), a sample panel of the facing materials to be used including for the plinth(s) and colonnade columns shall be erected on site to establish the detail of bonding, coursing and colour and type of jointing and shall be agreed in writing with the local planning authority. The quality of finish and materials incorporated in any approved sample panel(s), which shall not be demolished prior to completion of development of both 50 and 60 Station Road, shall be maintained throughout the development.

Reason: In the interests of visual amenity and to ensure that the quality and colour of the detailing of the brickwork/stonework and jointing is acceptable and maintained throughout the development. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

39. Prior to the commencement of the development hereby approved, with the exception of below ground works, full details of the proprietary roof glazing system including material(s), edge and flashing methods, etc. shall be submitted to and approved in writing by the local planning authority. Large-scale cross-section drawings may be appropriate to show details. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

40. Prior to the commencement of the development hereby approved, with the exception of below ground works, full details of glass type(s) to be used in curtain walling/windows/doors or other glazed features shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

41. Prior to the commencement of the development hereby approved, with the exception of below ground works, full details of non-masonry walling systems to be used shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

42. No metal-clad or other non-traditional roofs shall be erected until full details of such roofs including materials, colours, surface finishes and relationships to rooflights or other rooftop features have been submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

43. Prior to the commencement of development, full details of all external joinery [whether of metal, timber or hybrid construction] including frames, thresholds, mullions, transoms, finishes, colours, etc., shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

44. Prior to the commencement of development, with the exception of below ground works, full details of colonnade soffits shall be submitted to and approved in writing by the local planning authority. This may consist of large-scale drawings and/or samples. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

45. Prior to the commencement of the development hereby approved, with the exception of below ground works, full details of all coping to the walls shall be submitted to and approved in writing by the local planning authority. Large-scale cross-sectional drawings may be appropriate for depicting some details. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the building is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

46. Prior to commencement of occupation of either 50 or 60 Station Road a signage strategy for use in association with the occupation of 50 and 60 Station Road shall be submitted to and approved by the local planning authority in writing. The approved signage strategy shall thereafter be retained and all external signage shall conform to the strategy unless otherwise agreed in writing by the local planning authority.

Reason: To ensure that the appearance of the building is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

47. Prior to the commencement of the development hereby approved, with the exception of below ground works, full details, in terms of materials, fixing, surface finish & colour, of all metalwork [stairs, balustrades, grilles, railings, brackets, window cleaning gantries & associated equipment, columns, louvres, grilles, mesh or wire frames etc.] shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the building is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

48. Prior to the commencement of development, with the exception of below ground works, full details of external visible masonry brackets, clamps, restraints and other support systems shall be submitted to and approved in writing by the local planning authority. This may consist of large-scale drawings and/or samples. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

49. Notwithstanding the details shown on the approved plans, full details of the external treatment of the cycle store adjacent to the southern boundary shall be submitted to and approved in writing by the local planning authority prior to commencement of works to provide the cycle store. The development shall be implemented and maintained in accordance with the approved details

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

50. No rooftop plant shall be constructed on 50 Station Road until such time as full details, to a large scale, of any rooftop plant screening systems to be installed, where relevant, have been submitted to and approved in writing by the local planning authority. This may include the submission of samples of mesh/louver types and the colour(s) of the components. Colour samples should be identified by the RAL or BS systems. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the details of development are acceptable. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

51. No rooftop plant shall be constructed on 60 Station Road until such time as full details, to a large scale, of any rooftop plant screening systems to be installed, where relevant, have been submitted to and approved in writing by the local planning authority. This may include the submission of samples of mesh/louver types and the colour(s) of the components. Colour samples should be identified by the RAL or BS systems. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the details of development are acceptable. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

52. Prior to the commencement of development of 50 Station Road, with the exception of below ground works, full details of all solar panels [water pre-heat, etc.] and/or photovoltaic cells, including type, dimensions, materials, location, fixing, etc. shall be submitted to and approved in writing by the local planning authority. In bringing forward such details the applicant is encouraged to site such features so as not to be visible from ground level. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

53. Prior to the commencement of development of 60 Station Road, with the exception of below ground works, full details of all solar panels [water pre-heat, etc.] and/or photovoltaic cells, including type, dimensions, materials, location, fixing, etc. shall be submitted to and approved in writing by the local planning authority. In bringing forward such details the applicant is encouraged to site such features so as not to be visible from ground level. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

54. Prior to the commencement of occupation of 50 Station Road, a lighting plan including details of the height, type, position and angle of any external lighting shall be submitted to and approved in writing by the local planning authority. The development shall be implemented and maintained in accordance with the approved plan.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

55. Prior to the commencement of occupation of 50 Station Road, a lighting plan including details of the height, type, position and angle of any external lighting shall be submitted to and approved in writing by the local planning authority. The development shall be implemented and maintained in accordance with the approved plan.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 4/11)

INFORMATIVE: Highways - Buildings footings or basements must not extend out under the public highway except in the case of basements with the express permission of the Highway Authority and under licence. Adopted areas should also exclude areas under balconies except under licence (Section 177 of the Highways Act 1980)

INFORMATIVE: Highways - The applicant is advised that any granting of Planning Permission does not constitute a permission or licence to a developer to carry out any works within, or disturbance of, or interference with, the Public Highway, and a separate permission must be sought from the Highway Authority for such works.

INFORMATIVE: Highways - Notwithstanding any consent granted under the relevant planning act/s, the applicant is advised that before any works are carried out on any footway, carriageway, verge or other land forming part of the public highway the express consent of Cambridgeshire County Council as the Local Highway Authority will be required. All costs associated with any construction works will be borne by the developer. The developer will not be permitted to drain roof water over the public highway, nor across it in a surface channel, but must make arrangements to install a piped drainage connection. No window or door will be allowed to open over a highway and no foundation or footing for the structure will be allowed to encroach under the public highway.

INFORMATIVE: Highways - Notwithstanding any consent granted under the relevant planning act/s, the applicant is advised that before any works are carried out on any footway, carriageway, verge or other land forming part of the public highway the express consent of Cambridgeshire County Council as the Local Highway Authority will be required. All costs associated with any construction works will be borne by the developer.

INFORMATIVE: To satisfy conditions 27 and 28, the noise level from all plant and equipment, vents etc (collectively) associated with this application should not raise the existing background level (L90) by more than 3 dB(A) (i.e. the rating level of the plant needs to match the existing background level). This requirement applies both during the day (0700 to 2300 hrs over any one hour period) and night time (2300 to 0700 hrs over any one 5 minute period), at the boundary of the premises subject to this application and having regard to noise sensitive premises. Tonal/impulsive noise frequencies should be eliminated or at least considered in any assessment and should carry an additional 5 dB(A) correction. This is to guard against any creeping background noise in the area and prevent unreasonable noise disturbance to other premises.

It is recommended that the agent/applicant submits a noise prediction survey/report in accordance with the principles of BS4142: 1997 "Method for rating industrial noise affecting mixed residential and industrial areas" or similar. Noise levels shall be predicted at the boundary having regard to neighbouring premises.

Such a survey / report should include: a large scale plan of the site in relation to neighbouring premises; noise sources and measurement / prediction points marked on plan; a list of noise sources; details of proposed noise sources / type of plant such as: number, location, sound power levels, noise frequency spectrums, noise directionality of plant, noise levels from duct intake or discharge points; details of noise mitigation measures (attenuation details of any intended enclosures, silencers or barriers); description of full noise calculation procedures; noise levels at a representative sample of noise sensitive locations and hours of operation.

Any report shall include raw measurement data so that conclusions may be thoroughly evaluated and calculations checked.

INFORMATIVE: To satisfy conditions 23 and 24, details should be provided in accordance with Annex B of the, "Guidance on the Control of Odour and Noise from Commercial Kitchen Exhaust Systems," prepared by Netcen on behalf of Department for Environment, Food and Rural Affairs (DEFRA) dated January 2005 available at <http://www.defra.gov.uk/environment/quality/noise/research/kitchenexhaust/documents/kitchenreport.pdf>

INFORMATIVE: Food Safety - As the premises are intended to be run as a food business the applicant is reminded that under the Food Safety Act 1990 (as amended) the premises will need to be registered with Cambridge City Council. In order to avoid additional costs it is recommended that the applicant ensure that the kitchen, food preparation and food storage areas comply with food hygiene legislation, before construction starts. Contact the Food and Occupational Safety (FOS) Team of the Refuse and Environmental Service at Cambridge City Council on telephone number (01223) 457890 for further information.

INFORMATIVE: Licensing - If the premises are intended to provide alcohol, regulated entertainment or food after 11pm or before 5 am they may require a Premise Licence under the Licensing Act 2003. The applicant is advised to contact The Licensing Team of Refuse and Environmental Service at Cambridge City Council on telephone number (01223) 457899 for further information.

INFORMATIVE: Meeting the needs of disabled people

In order to meet the needs of disabled people, the applicant is recommended to take the following advice into account in the detailed design of the building:

The main door should be automated. Side doors alongside revolving doors would best be electrically opening or asymmetrical of which one is at least 900mm.

Reception and cafe bars need dropped height counters and hearing loop.

INFORMATIVE: New development can sometimes cause inconvenience, disturbance and disruption to local residents, businesses and passers by. As a result the City Council runs a Considerate Contractor Scheme aimed at promoting high standards of care during construction. The City Council encourages the developer of the site, through its building contractor, to join the scheme and agree to comply with the model Code of Good Practice, in the interests of good neighbourliness. Information about the scheme can be obtained from The Considerate Contractor project Officer in the Planning Department (Tel: 01223 457121).

INFORMATIVE: Advice from the Environment Agency

Advice to Applicant:

Contaminated soil that is, or must be disposed of, is waste. Therefore, its handling, transport, treatment and disposal is subject to waste management legislation, which includes:

Duty of Care Regulations 1991

Hazardous Waste (England and Wales) Regulations 2005

Environmental Permitting (England and Wales) Regulations 2010

The Waste (England and Wales) Regulations 2011

The CLAIRE Definition of Waste: Development Industry Code of Practice (version 2) provides operators with a framework for determining whether or not excavated material arising from site during remediation and/or land development works are waste or have ceased to be waste. Under the Code of Practice:

excavated materials that are recovered via a treatment operation can be re-used on-site providing they are treated to a standard such that they are fit for purpose and unlikely to cause pollution

treated materials can be transferred between sites as part of a hub and cluster project

some naturally occurring clean material can be transferred directly between sites.

Developers should ensure that all contaminated materials are adequately characterised both chemically and physically, including in line with British Standards BS EN 14899:2005 'Characterisation of Waste - Sampling of Waste Materials - Framework for the Preparation and Application of a Sampling Plan' for material to be removed from site, and that the permitting status of any proposed treatment or disposal activity is clear. If in doubt, the Environment Agency should be contacted for advice at an early stage to avoid any delays.

If the total quantity of waste material to be produced at or taken off site is hazardous waste and is 500kg or greater in any 12 month period the developer will need to register with us as a hazardous waste producer. Refer to our website at www.environment-agency.gov.uk for more information.

Land contamination investigations should be carried out in accordance with BS 5930:1999-2010 'Code of Practice for site investigations' and BS 10175:2011 'Investigation of potentially contaminated sites - Code of Practice' as updated/amended. Site investigation works should be undertaken by a suitably qualified and experienced professional. Soil and water analysis should be fully MCERTS accredited.

Soakaways and other infiltration Suds must not be constructed in contaminated ground. The use of infiltration drainage would only be acceptable if a phased site investigation showed the presence of no significant contamination. The use of non infiltration Suds may be acceptable subject to our agreement. We would need to be consulted on the results of the site investigation and on any protection measures.

The maximum acceptable depth for infiltration Suds is 2.0 m below ground level, with a minimum of 1.2 m clearance between the base of infiltration Suds and peak seasonal groundwater levels. We consider that deep bore and other deep soakaway systems are not appropriate in areas where groundwater constitutes a significant resource (that is where aquifer yield may support or already supports abstraction). Deep soakaways increase the risk of groundwater pollution. See our Groundwater Protection GP3 documents, particularly Part 4 P4-7, for further information.

Please also see our advice to the LPA on land contamination.

We recommend that developers should:

1. Follow the risk management framework provided in CLR11, 'Model Procedures for the Management of Land Contamination', when dealing with land affected by contamination;
2. Refer to our "Guiding Principles for Land Contamination" for the type of information that we require in order to assess risks to controlled waters from the site. The Local Authority can advise on risk to other receptors, for example human health;
3. Refer to our "Verification of Remediation of Land Contamination" report;
4. Refer to our "Groundwater Protection: policy and practice (GP3)" documents (<http://www.environment-agency.gov.uk/research/library/publications/40741.aspx>);
5. Refer to our 'Position Statement on the Definition of Waste: Development Industry Code of Practice'; and
6. Refer to our website at www.environment-agency.gov.uk for more information.

Surface Water Drainage:

All surface water from roofs shall be piped direct to an approved surface water system using sealed downpipes. Open gullies should not be used.

Only clean, uncontaminated surface water should be discharged to any soakaway, watercourse or surface water sewer.

General Informatives:

Any culverting or works affecting the flow of a watercourse requires the prior written Consent of the Environment Agency under the terms of the Land Drainage Act 1991/Water Resources Act 1991. The Environment Agency seeks to avoid culverting, and its Consent for such works will not normally be granted except as a means of access.

The granting of planning approval must not be taken to imply that consent has been given in respect of the above.

Foul Water Drainage:

All foul sewage or trade effluent, including cooling water containing chemical additives, or vehicle washing water, including steam cleaning effluent shall be discharged to the public foul sewer with the prior approval of Anglian Water services.

Pollution Control:

Surface water from roads and impermeable vehicle parking areas shall be discharged via trapped gullies.

Prior to being discharged into any watercourse, surface water sewer or soakaway system, all surface water drainage from lorry parks and/or parking areas for fifty car park spaces or more and hardstandings should be passed through an oil interceptor designed compatible with the site being drained. Roof water shall not pass through the interceptor.

Surface water drainage from covered or underground parking areas shall be discharged to the public foul water sewer with the prior approval of Anglian Water Services.

Drainage from open parking areas that will discharge, directly or otherwise, to a surface watercourse must be first passed through an oil interceptor. The Environmental Permitting Regulations make it an offence to cause or knowingly permit any discharge that will result in the input of pollutants to surface waters.

Site operators should ensure that there is no possibility of contaminated water entering and polluting surface or underground waters

INFORMATIVE: This planning permission should be read in conjunction with its associated deed of planning obligation prepared under s.106 of the Town and Country Planning Act 1990 (as amended)

Reasons for Approval

1. This development has been approved, conditionally, because subject to those requirements it is considered to generally conform to the Development Plan, particularly the following policies:

East of England Plan policies: SS1 SS3 E3 T1 T2 T4 T9 T13
T14 T15 ENV6 ENV7 WM6 CSR1 CSR2

Cambridgeshire and Peterborough Structure Plan policies: P6/1
P9/8 P9/9

Cambridge Local Plan (2006): 3/1 3/4 3/6 3/7 3/11 3/12 3/13 4/4
4/11 4/12 4/13 4/14 4/15 5/11 7/2 8/2 8/4 8/6 8/9 8/10 8/16 8/18
9/1 9/9 10/1

2. The decision has been made having had regard to all other material planning considerations, none of which was considered to have been of such significance as to justify doing other than grant planning permission.

These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.

2. Unless prior agreement has been obtained from the Head of Planning, in consultation with the Chair and Spokesperson of this Committee to extend the period for completion of the Planning Obligation required in connection with this development, if the Obligation has not been completed by 30 September 2013, or if Committee determine that the application be refused against officer recommendation of approval, it is recommended that the application be refused for the following reason(s):

The proposed development does not make appropriate provision for transport mitigation measures/infrastructure provision, mitigation of potential for overspill parking, the funding and agreement of the a Travel Plan Co-Ordinator, public art, relocation of a community facility, restriction on occupation of offices and monitoring in accordance with Cambridge Local Plan 2006 policies 3/7, 5/11, 7/2, 8/2, 8/3, 9/9 and 10/1, Cambridgeshire and Peterborough Structure Plan 2003 policies P6/1, P9/8 and P9/9 and as detailed in the Planning Obligation Strategy 2010, the Public Art Supplementary Planning Document 2010 and the Southern Corridor Area Transport Plan 2002.

In the event that the application is refused, and an Appeal is lodged against the decision to refuse this application, delegated authority is sought to allow officers to negotiate and complete the Planning Obligation required in connection with this development.



Keywords: child sexual abuse; disclosure; social support

US MASTERPLAN BOUNDARY

55-60 STATION ROAD SITE BOUNDARY

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50 & 60 STATION ROAD

Brookgate, Cambridge

Keywords: child sexual abuse; disclosure; legal system; mental health

TABLE 1

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Figure 1. The study area and the location of the sampling stations.

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GENERAL ARRANGEMENT LOCATION PLAN

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50 & 60 STATION ROAD

Cambridge, UK

Brinkley, Cambridge

10/01/2012

10/01/2012

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10/01/2012

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GENERAL ARRANGEMENTS

GROUND FLOOR PLAN

10/01/2012

10/01/2012

10/01/2012

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10/01/2012

provision of inter-cover external cycle parking

10028 GA-L-100 E



- 2007/12 FOR PLANNING
- 2007/12 FOR PLANNING

Brookgate, Cambridge



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www.grmshaw-architects.com

GENERAL ARRANGEMENT ELEVATION
NORTH

26-01-2012	1.200,00 A
VV	415

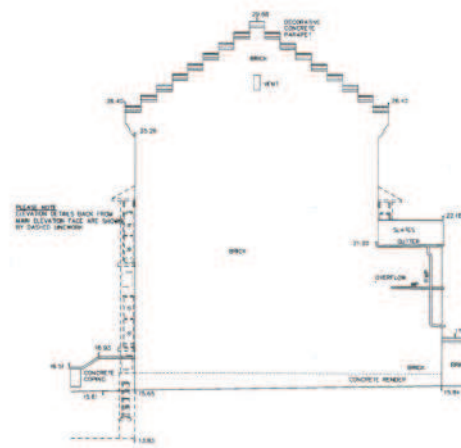
Change in parapet height

10028 GA-L-130



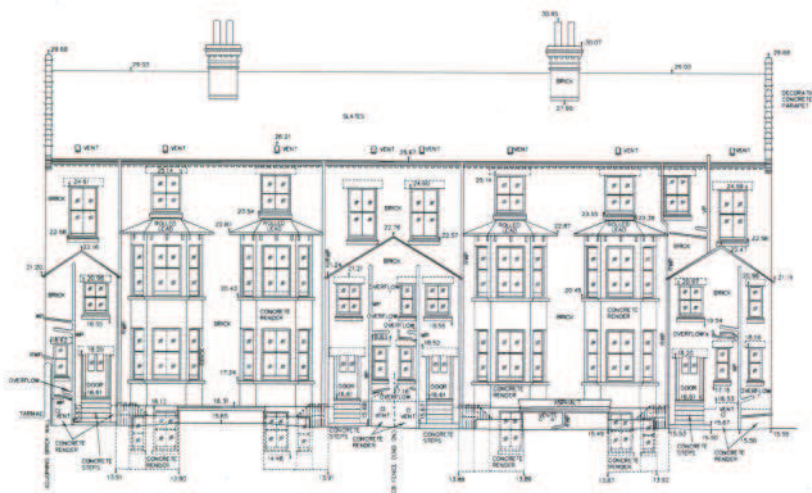
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NORTH ELEVATION



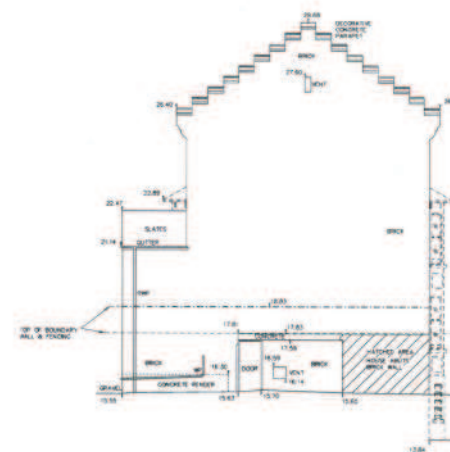
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WEST ELEVATION



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SOUTH ELEVATION



DATUM 10.00m AOD

EAST ELEVATION

NOTES

- The accuracy and content of this drawing and information on the original specification and CD should be confirmed before use in any other way or use in CD form.
- Where underground services are shown, all responsibility for their location shall remain with the client. The signed specification and specification. Before use of this information the user should ensure that all services are shown and that the information is correct and that the user is not liable for any damage or loss caused by the use of this information. Due to the nature of this work and the information required by the client, the user should ensure that the information is correct and that the user is not liable for any damage or loss caused by the use of this information.
- All dimensions are to be taken from the centre of the building unless otherwise stated.
- All levels and coordinates are given in the datum specified in the specification.

LEGEND

SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
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ED I SURVEYS LTD.

100-05, HANLEY ROAD, SPENCER, STAFFS, ST4 6JH

01927 551111 Fax 01927 551112 Email edis@ediconsurveys.co.uk

PROJECT

10-10, HANLEY ROAD, SPENCER, STAFFS, ST4 6JH

CLIENT

10-10, HANLEY ROAD, SPENCER, STAFFS, ST4 6JH

DATE

10-10, HANLEY ROAD, SPENCER, STAFFS, ST4 6JH

SCALE

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STATUS

10-10, HANLEY ROAD, SPENCER, STAFFS, ST4 6JH

REVISIONS

NO.	DATE	DESCRIPTION
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PLANNING COMMITTEE

Date: 6th March 2013

Application Number	12/1553/CAC	Agenda Item	
Date Received	10th December 2012	Officer	Mrs Sarah Dyer
Target Date	4th February 2013		
Ward	Trumpington		
Site	32 - 38 Station Road Cambridge Cambridgeshire CB1 2JH		
Proposal	Demolition of 32-38 Station Road.		
Applicant	Mr Sven Topel c/o Agent		

SUMMARY	<p>The development accords with the Development Plan for the following reasons:</p> <p>An assessment of 32-38 Station Road as a Building of Local Interest (BLI) and a heritage asset has been carried out.</p> <p>The demolition of 32-38 Station Road is justified by the public benefit that will derive from the development of the site for office accommodation as part of the wider Station Area redevelopment</p> <p>This revised application addresses the reasons for refusal that were agreed by Planning Committee in relation to application reference 12/0496/CAC</p>
RECOMMENDATION	APPROVAL

0.0 INTRODUCTION

- 0.1 This application and the associated application for planning permission for a pair of new office buildings follow on from a refusal of planning permission and CAC by the Planning Committee in July 2012. This decision was taken against officer recommendation and is the subject of an appeal to the

Planning Inspectorate which is to be held by Public Inquiry in May/June this year.

- 0.2 The reasons for refusal of the planning application are set out in the introduction to my report for that application elsewhere on the Agenda. The Conservation Area consent application (Ref. 12/0496/CAC) was refused for the following reasons:
1. The proposed demolition is contrary to policies 4/11 and 4/12 of the Cambridge Local Plan (2006) and paragraph 136 of the National Planning Policy Framework 2012 in that in the absence of an approved redevelopment scheme that has a contract for redevelopment and which preserves and enhances the character or appearance of the Conservation Area by faithfully reflecting its context or providing a contrast with it, the demolition of the buildings would result in the loss of a heritage asset in the form of Buildings of Local Interest which contribute positively to the character and appearance of the Conservation Area.
 2. The public benefit from the development fails to provide sufficient justification for the demolition of Buildings of Local Interest, which are recognised as heritage assets. The development is therefore contrary to policy 4/12 of the Cambridge Local Plan 2006 and to guidance provided by the National Planning Policy Framework.
- 0.3 These previous decisions are a very significant material consideration in the assessment and determination of the current applications. For this reason my Report focusses on the key differences between the two schemes and whether the revised proposals overcome the reasons for refusal that are set out above. It would be unwise to reject the current applications on grounds that were regarded as acceptable to the Council in relation to the first application unless there are demonstrable changes in circumstances.
- 0.4 The current planning application concerns a revised scheme which in the view of the applicants overcomes the earlier reasons for refusal. The key areas of difference between the refused scheme and the current scheme are in terms of the design of the building, the amount of car parking and a fuller explanation of the public benefits that justify the demolition of

32-38 Wilton Terrace, which are Buildings of Local Interest (BLI). This application for Conservation Area Consent is supplemented by additional information to justify the demolition of the buildings. The proposal to demolish both buildings remains unchanged.

- 0.5 With the exception of the revocation of the East of England Plan the planning policy context against which a decision must be made is unchanged in comparison with that which applied in July 2012. The following report describes and assesses the application for Conservation Area Consent.

1.0 SITE DESCRIPTION/AREA CONTEXT

- 1.1 The application site forms part of a larger area, which is the subject of the CB1 Station Area Redevelopment proposals for which outline planning permission was granted in April 2010. The site lies on the south side of Station Road and to the east of the access serving the Warren Close development. The western half of the site is occupied by 32-38 Station Road (Wilton Terrace) which accommodates Woodlands Doctors Surgery and Brookgate's Offices (the applicants). The eastern half of the site was previously occupied by offices serving the Rank Hovis site that have been demolished as part of the CB1 development. This part of the site is currently in use as a contractor's compound.
- 1.2 To the south of the site is the Warren Close housing development and land which is under development for flats with commercial space at ground level. A six storey block of flats at Warren Close sits behind the western half of the site and following the completion of development a public square and a seven storey block of flats will sit behind the eastern half. To the west the site is bounded by the access road serving Warren Close beyond which is office buildings. To the east is Murdoch House a three storey office block with undercroft which fronts the Station. To the north the site is bounded by Station Road beyond which is former Red House site that has planning permission for a hotel and the current station cycle park.
- 1.3 The site is within the Station Area Redevelopment Framework Boundary and within the Central Conservation Area No.1. 32-38 Station Road are buildings of Local Interest as are the Mill and

Silo that sit to the southeast. The Station is a listed building. The site falls within the controlled parking zone.

2.0 THE PROPOSAL

2.1 Conservation Area Consent is sought for the demolition of the terrace to facilitate the development of the western half of a pair of office buildings. Parameter Plan 1 of the Outline consent identifies 32-38 Station Road as Buildings of Local Interest that are to be demolished. Whilst this does not negate the need for Conservation Area Consent it is a material consideration.

2.2 The application is accompanied by the following supporting information:

- ☐ Design and Access Statement by Grimshaw
- ☐ Heritage Statement by Beacon Planning
- ☐ Planning Statement by Savills

3.0 SITE HISTORY

Reference	Description	Outcome
05/1166	Conservation Area Consent for demolition buildings on the Rank Hovis site	A/C
06/0266/OUT	CB1 Station Area Redevelopment	A/C
09/0031	Conservation Area Consent for demolition buildings on the Rank Hovis site	A/C
11/1303/FUL	Demolition of 32 – 38 Station Road and erection of two office buildings	Withdrawn
11/1351/CAC	Conservation Area Consent for demolition of 32-38 Station Road	Withdrawn
12/0502	Demolition of 32 – 38 Station Road and erection of two office buildings	Refused

12/0496	Conservation Area Consent for demolition of 32-38 Station Road	Refused
	Non Material Amendment for Realignment of SAR	Approved
12/1556	Demolition of 32 – 38 Station Road and erection of two office buildings	Pending determination

4.0 **PUBLICITY**

4.1	Advertisement:	Yes
	Adjoining Owners:	Yes
	Site Notice Displayed:	Yes
	Public Meeting/Exhibition	No
	DC Forum	No

5.0 **POLICY**

5.1 See Appendix 1 for full details of Central Government Guidance, Cambridgeshire and Peterborough Structure Plan 2003 policies, Cambridge Local Plan 2006 policies, Supplementary Planning Documents and Material Considerations.

5.2 Relevant Development Plan policies

PLAN	POLICY NUMBER
Cambridge Local Plan 2006	4/11 4/12 4/13 8/2 9/9

5.3 Relevant Central Government Guidance, Supplementary Planning Documents and Material Considerations

Central Government Guidance	National Planning Policy Framework March 2012
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	<p>Circular 11/95</p> <p>Community Infrastructure Levy Regulations 2010</p>
Material Considerations	<p><u>Central Government:</u></p> <p>Letter from Secretary of State for Communities and Local Government (27 May 2010)</p> <p>Written Ministerial Statement: Planning for Growth (23 March 2011)</p>
	<p><u>Citywide:</u></p> <p>Air Quality in Cambridge – Developers Guide</p>
	<p><u>Area Guidelines:</u></p> <p>Buildings of Local Interest</p> <p>Station Area Development Framework/Station Area Conservation Appraisal</p>

6.0 CONSULTATIONS

Cambridgeshire County Council (Engineering)

- 6.1 No objection subject to a condition to secure a Traffic Management Plan for demolition/construction traffic and informatives.

Head of Environmental Services

- 6.2 Environmental Protection

Construction Method Statement

During construction and demolition, noise, vibration and dust has the potential to harm the locality amenity if not controlled. A Demolition and Construction Environmental Management Plan (DCEMP) should be secured by condition.

Urban Design and Conservation Team

- 6.3 The existing terraced houses (32-38 Station Road) are Buildings of Local Interest (BLI), in some ways, are pretty typical of their type and era and common enough in many larger towns and cities across Britain. These examples are slightly more decorative than is usual in Cambridge, with the 'crow-stepped' gables and red brick banding, quoins and so on. This may result from the locally well-known architect, Richard Reynolds Rowe, who did use such details and operated in the area and, if this could be indisputably proved, would give rather more weight to the history of the terrace. Nonetheless, these houses are not particularly rare architecturally and have lost some of their residential character through changes-of-use, particularly by the unfortunate and visually prominent ramp occupying the front garden space of the surgery. The other item of interest is the 'no fines'-type concrete boundary wall that also occurs in front of the villas further down Station Road and again in Warkworth Street & Terrace nearby. This is believed to be a very early use of concrete but not enough research has been done to establish the rarity or historic value of these examples.

The applicant's agents have submitted an Heritage Statement which covers the history of the area in some detail and analyses the 'significance' of the various components of the BLIs and the CA. There is broad agreement with the content in those respects.

This application relates to a replacement building of a slightly different design to the previous application, so the discussion of the proposal to demolish the existing terrace to clear the site remains very similar to the previous.

"Whilst the 'masterplan' process always assumed demolition of these BLIs, no consent has ever been granted and the changed circumstances since 2008 and changes to the proposals for this area of the larger scheme mean that this CAC application must be assessed against current policies. It also is tied inextricably to the application for the replacement building and the two things must be seen, assessed and balanced together. The houses are undoubtedly 'heritage assets' although of fairly modest significance in themselves and changes to the surrounding area in recent decades have disconnected them

from the nearby residential areas of similar age and scale. They were also in the same area as, but of a different character to, the railway lands & buildings nearby [and the industry co-located deliberately] and hence the station building [the LB itself]. Once the character of the area was changed substantially by the demolition for and construction of the so-called 'Deity' office blocks, the residential nature became confined to the other side of the street largely and the gradual drift of industry away from the now mainly passenger railway allowed for the comprehensive redevelopment proposed by the 'masterplan'. Whilst the substantially different character to the area already being formed [by the Microsoft HQ building, for example] will contrast strongly with these BLIs, that is not – in itself – a reason for demolition; indeed the opposite might be argued as the whole scheme is likely to take years to complete and there will be times during redevelopment that noticeable contrast in building ages, types and scales will be evident. And there will be, or should be, a marked difference in age, type and scale between the LB and its new setting for the foreseeable future and nobody seems to have suggested that was inappropriate.”

However, the 'masterplan' always envisaged this side of the street to be commercial in nature and of a much larger scale and that intent would have been fully aired at the 'outline' application stage. The terraced houses are not of great architectural or historic significance although they have a pleasant, modest character but are not particularly unusual in the wider context. So the policies requiring justification for demolition in CAs need to be tested against the 'public benefit' from redevelopment

If the BLIs are to be demolished, then the quality and suitability of the proposed replacement building(s) both for the CA and the nearby LB must be assessed before any scheme can be supported. The detailed assessment by U D & C Section is to be found at 12 /1556 / FUL on this matter.

Conclusion:

The loss of BLIs in a prominent location in the CA is always a matter for regret and the policies at national & local level are against such a loss except in cases where there is a demonstrable benefit to be derived. In this case, the decision

makers who granted outline permission for the 'masterplan' were aware of the presumed demolition and assessed it to be worthwhile. All the discussions about the replacement building were predicated on the design having to be assessed as of suitable quality to meet the policy 'tests'.

English Heritage

6.4 Comments as previous applications:

Summary

The principle of demolition of 32-38 Station Road was given tacit support when the CB1 Masterplan was agreed. However consent should only be granted once a scheme for high quality replacement buildings has been secured. The current proposals require changes to the Masterplan which have much to recommend them and the revised design has gone a long way to address the previous concerns raised by English Heritage.

Conservation Area Consent application

The NPPF includes a provision of a presumption in favour of the conservation of heritage assets. In this instance 32-38 Station Road are not designated heritage assets but the Conservation Area within which they are located is and therefore the presumption in favour of conservation is relevant. The removal of these buildings will enable the implementation of a holistic redevelopment that will deliver an overall enhancement of the Conservation Area whereas to retain them would be at odds with the new context.

The Victorian Society

6.5 The Victorian Society was not formally consulted on the application because 32-38 Station Road are not listed buildings. No comments have been received in relation to the current applications. The following comments were made previously:

The Society objects to the demolition of 32-38 Station Road. They are a striking late Victorian terrace and may have been designed by Richard Reynolds-Rowe. The rarity of the buildings adds to their value in the streetscape.

The poor state of the frontage and the access ramp can be easily rectified. Other sites could provide new office space which would negate the need to demolish these buildings.

The Council has an obligation to ensure that development either preserves or enhances the Conservation Area.

Design and Conservation Panel (Meeting of 14 March 2012)

6.6 The conclusions of the Panel meeting were as follows:

In strategic terms, the Panel considers that the new approach is a great improvement. The change in massing, the handling of the frontage at ground level along Station Road and the greater animation of the frontage to the 'anti-chamber' square to the south are welcomed. The 'kit of parts' proposed for the elevations looks promising but further refinement of the design is still needed, as is further examination of the treatment of stair cores at ground level.

VERDICT –

1. The strategy of the massing, the relationship of the stair core with Station Road, the overall strategy for the elevations and the handling of the public realm, GREEN (5), AMBER (1)

2. The 'kit of parts' for the elevations, the handling of the elevations at ground level, the design of the 'fins' and other components and the planting of the terraced area, GREEN (3), AMBER (2)

The relevant section of the minutes of the panel meeting(s) are attached to this report as Appendix A.

Design and Conservation Panel (Meeting of 13 February 2013)

6.7 The minutes of the Panel meeting were as follows:

Following the last presentation in March 2012 and the refusal in July of the previous application (12/0502/FUL), the Panel were invited to review the amendments made to the scheme, which is the subject of the current application. The amendments include;

the pushing back of the facade of No 50 to reduce its apparent bulk and the inclusion of an additional 15 car parking spaces at basement level. The Panel were told that the application is to be recommended for approval at Planning Committee in early March.

The Panel's comments are as follows:

- Set back of No. 50. The Panel welcome the setting back by 3 metres of the facade of No.50. It was felt that this new configuration had several advantages over the previous flush alignment of the two buildings. The benefits arising from the adjustment to the building's alignment were felt to include:-
 - o the introduction of an element of ascending progression injected more interest to the streetscape and sets off the café space more effectively,
 - o had created more 'breathing space' at pavement level which could lead to less potential conflict between pedestrians and cyclists,
 - o allowed for more flexibility regarding the choice of street trees
 - o the generation of an 'implied' space shared with buildings on the opposite side of Station Road.
- Stonework fins. In the Panel's view, the proposed application of the simple components, sometimes projected, rotated and flush was playful and created interest. However, the Panel commented that careful detailing is necessary to prevent the kind of unwelcome shadowing caused by the accumulation of dirt between the joints.
- Entrance (No 50). The Panel are comfortable with the logic of the new entrance. The addition of an etched '50' into the stonework would, in the Panel's view, introduce a playful typographical element and enhance the entrance.
- Additional parking spaces. The pressure to provide additional car parking in order to make the buildings more marketable is understood, and the Panel welcome the determination to deliver the extra spaces without

compromising the design.

- Potential wider benefit arising from the demolition of Wilton Terrace (32-38 Station Road). Within the terms of the National Planning Policy Framework, the Panel felt that the removal of the heritage asset is justified as it would enable the implementation of the CB1 Masterplan as a whole, and enhance the Conservation Area.

Conclusion.

The subtle changes deliver a significantly enhanced scheme. A better relationship between the two buildings and the adjoining space has resulted from the move away from them being a matched pair to becoming a *related* pair. The retention of 32-38 Station Road was felt to be at odds with the new context set by the implementation of a holistic redevelopment.

VERDICT – GREEN (unanimous)

- 6.8 The above responses are a summary of the comments that have been received. Full details of the consultation responses can be inspected on the application file.

7.0 REPRESENTATIONS

22 Brooklands Avenue
5 Clarendon Road
9 Clarendon Road
17 Clarendon Road (x4)
33 Glisson Road
2 Highsett, Hills Road
6 Highsett, Hills Road
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60 Highsett, Hills Road
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62 Highsett, Hills Road
69 Highsett, Hills Road
75 Highsett, Hills Road
82 Highsett, Hills Road
59 Mawson Road
21 Panton Street
2 Saxon Street

15 Shelly Garden
25 Tenison Avenue
10 The Lawns, Clerk Maxwell Road
2 Vintner Terrace
1 Warren Close
Whitlocks, High Street, Trumpington

7.2 The representations can be summarised as follows:

Concerns and objections

Previous proposals and current applications

The proposals are not substantially different and the revisions make no difference.

Both applications should be refused.

Loss of Existing Buildings

The existing buildings with their architectural heritage and elegant facades should not be demolished.

The existing buildings are an important example of an earlier age of fine architecture which should be preserved.

Wilton Terrace has been designed to relate to the Station.

Demolition of the existing buildings amounts to vandalism against the wishes of residents.

Loss of welcoming vista along Station Road.

The terrace does not need to be demolished and could be incorporated into the new development.

Renovation of the buildings would be more sustainable.

Have planners relied on reports on architectural value of Wilton Terrace by the applicants or have they commissioned an independent view?

The terrace will not clash with other buildings but will provide a historically relevant backdrop.

- 7.3 Brooklands Avenue Area Residents Association has made representations as follows:

Demolition of existing buildings

We object to the demolition of the present Victorian terrace on the site (BLIs), and believe that it should be preserved, as architecturally in keeping with the properties on the other side of Station Road and of the Station itself. In this connection we further note that the houses are designated as “Buildings of Local Interest.” As indicated above, the case for demolition cannot be deemed as having been agreed, since as a full application this has to be considered from scratch, and cannot ride on the back of the Masterplan outline. Indeed there is a strong argument for retaining the properties in the interest of balance with the similar buildings on the north side of Station Road, and thereby preserving important features of this part of the Central Conservation Area.

Comparison with earlier refused applications

Insufficient clear public benefit to compensate for loss of a heritage asset – note ‘clear public benefit’ and not ‘clear financial benefit to the developer’. No such clear public benefit has been demonstrated.

For all the above reasons we urge the City Council to reject the proposed applications.

- 7.4 David Campbell Bannerman MEP has also made objections that relate to the loss of 32-38 Station Road. He objected to the previous application and does not believe that the application has notably changed. Following the previous Committee meeting he thought that a compromise was possible to include the retention of the building and that several Councillors agreed with this. He considers that the loss of these buildings and the new development to be harmful to the Conservation Area. He considers that the existing buildings have a evidential, aesthetic and communal value. In his view the level of harm caused by the loss of 32-38 Station Road heavily outweighs the benefits gained from the replacement building.

- 7.5 The above representations are a summary of the comments that have been received. Full details of the representations can be inspected on the application file.

8.0 ASSESSMENT

- 8.1 From the consultation responses and representations received and from my inspection of the site and the surroundings, I consider that the main issues are:

1. Principle of demolition
2. Impact on Heritage Assets
3. Other environmental impacts
4. Third party representations
5. Planning Obligation Strategy

Principle of Demolition

Parameter Plan 1 of the Masterplan indicates 32-38 Station Road as a group of Buildings of Local Interest that are to be demolished. In the report that was considered by Planning Committee in October 2008 the following comment is made about the demolition of these buildings:

‘The applicant has not provided a full justification for the demolition of 32-38 Station Road that will be necessary when an application is made for Conservation Area Consent for its demolition. Clearly Block I2 cannot be developed without removing 32-38 Station Road. The pivotal role that I2 has in the scheme is considered more fully below but essentially I would conclude that the loss of 32-38 Station Road is justified by the need to increase the density of development across the site in order to achieve the aim of improving the transport interchange. 32-38 Station Road are not worthy of listing and in my view to seek to refuse the masterplan on the grounds that these buildings should be retained alone would be very difficult to substantiate at appeal.’

My views have not changed. I consider that subject to the grant of Conservation Area Consent, the principle of the demolition of 32-38 Station Road is acceptable. I have dealt with the loss of the doctor’s surgery as a community facility in my report for the full planning application for the new office buildings.

Impact on Heritage Assets

- 8.2 In this section of my report I consider the submissions made by the applicant and make my assessment against the policy context. The applications for Conservation Area Consent and planning permission are supported by a Heritage Statement as required by paragraph 128 of the National Planning Policy Framework (NPPF). Officers in the Urban Design and Conservation Team have not raised any concerns about this analysis and support the scheme subject to the imposition of planning conditions to address matters of detail.

The applicants submissions

- 8.3 The applicants have correctly identified the heritage assets that are affected by the development as 32-38 Station Road which are Buildings of Local Interest (BLIs) and the Conservation Area including the Station and associated buildings, the villas on the north side of Station Road and the Mill. The NPPF includes buildings that are locally listed in the definition of a heritage asset.
- 8.4 The significance of 32-38 Station Road has been assessed using the NPPF and English Heritage's document Conservation Principles, Policies and Guidance 2008. The aspects of moderate significance have been identified as the evidential value as example of a late Victorian terrace which has been the subject of removal of some features of interest and the aesthetic value of the front elevation.

Aspects of low significance included the historical value, communal value, the south/east/west elevations, the interior and the setting. No elements of high significance were recognised.

The level of significance of this part of the Conservation Area is considered by the applicant to be moderate. This is because 32-38 Station makes a moderate contribution to the character.

- 8.5 Most of the Heritage Statement is unchanged in comparison with the document that supported the previous application. This is because the history of the site, the significance of the existing buildings, the character of the Conservation Area and relevant heritage policy has not changed. However there are

two key areas where changes have been made to the Heritage Statement. These relate to the impact of the proposed development on significance and the public benefits that will arise from demolition of the buildings and redevelopment of the site.

- 8.6 The new section in the Heritage Statement which considers the impact of the revised proposals heritage assets concludes that the proposed building will not have an adverse impact. This conclusion is supported by officers. The biggest change to the Heritage Statement is the emphasis that is made of the public benefits which will be achieved if the site is redeveloped. This part of the document has been expanded in direct response to the concerns raised by Committee that were articulated in the reasons for refusal of the previous Conservation Area Consent application.
- 8.7 The Heritage Statement addresses the following issues and makes the following observations about public benefits arising from the CB1 development:

Historical Background

The Station Area Development Framework (SADF) proposed the retention of Wilton Terrace but this was recognised as problematic when the detailed masterplan for the area was prepared.

The Rogers Masterplan

The Masterplan which formed the basis of the Outline consent was prepared between 2005 and 2008. The Environmental Statement that supported the Outline application included a Built Heritage section and companion volume that concluded that the loss of Wilton Terrace was justifiable on the grounds that the building has lost its context and was quite difficult to convert to modern commercial or residential use. The replacement of the terrace was recognised as an opportunity to allow a strong pattern of development to be created along this side of Station Road and would contribute towards the wider public benefits arising from comprehensive redevelopment.

Support for the Urban Design Strategy

The Masterplan provides a very clear urban design structure. In relation to Block I2 this was to be the pinnacle in terms of building height but was also to be set forward to create a visual pinch point and focus on the station building. The retention of Wilton Terrace completely undermines the urban design strategy.

Station Road's Evolving Character

Individual sites along the South side of Station Road are being brought forward. For example the approved Block J2 sets a pattern for the other J blocks. The proposed building will sit well with this character.

The Bigger Picture

When the Masterplan was approved it was anticipated that improvements to the Station as a transport interchange would be delivered as part of a comprehensive redevelopment of the area. However the economy has changed and it is no longer possible to deliver the development in one phase or in a series of large parcels. Development opportunities are now more dependent on dates of tenancy renewals and identification of future occupiers. This means it is easy to lose sight of the bigger picture and the contribution that individual sites make to the resources available for new infrastructure.

The Heritage Statement makes it clear that leaving Wilton Terrace in place or upgrading/extending it will not generate enough money to support the key drivers of the scheme i.e. the key public benefits.

- 8.8 In my view the approach that has been taken in the Heritage Statement to looking back at the rationale behind the redevelopment of Wilton Terrace and making a very clear statement about the importance of the redevelopment of the site in the wider context is very helpful. This information did not form such a robust part of the previous applications and in my view does help to overcome the second reason for refusal of the earlier Conservation Area Consent application.

The relevant policy context and assessment

- 8.9 The policy context for the determination of the application is unchanged from that which prevailed when the earlier applications were determined. The advice in the NPPF on dealing with the removal of heritage assets is as follows:

Where a proposed development will lead to substantial harm to or total loss of significance of a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or loss is necessary to achieve substantial public benefits that outweigh that harm or loss, or all of the following apply:

- *the nature of the heritage asset prevents all reasonable uses of the site*
- *no viable use of the heritage asset itself can be found in the medium term through appropriate marketing that will enable its conservation*
- *conservation by grant-funding or some form of charitable or public ownership is demonstrably not possible*
- *the harm or loss is outweighed by the benefit of bringing the site back into use.*

- 8.10 Local Plan policy is not silent on the issue of demolition of BLIs as heritage assets and requires that the demolition will only be permitted if the building is demonstrably incapable of beneficial use or reuse or there are clear public benefits arising from the development. The advice in both the policy and the NPPF helps to identify how an assessment of the application should be made.

- 8.11 Both the policy and the NPPF look to establish either a clear public benefit or compliance with a set of criteria. It is important to note that a justification for demolition does not need to establish both a public benefit argument and compliance with the criteria or test established by policy/guidance.

8.12 In this case the nature of the heritage asset does not prevent use of the site and the buildings are currently in beneficial use. No evidence has been brought forward regarding the availability of grant funding or public/charitable ownership. The justification in this case is solely reliant upon the 'public benefit' argument.

8.13 Although Conservation Area Consent is needed I would argue that the basis of the public benefit was established at the Outline Consent stage. The redevelopment of the Station Area as a whole is necessary to bring forward the very significant improvements to the transport interchange that include works to the Station, the new Station Square, the bus interchange and Cycle Park. An increased density of development is needed to support these improvements. This site is the key to the delivery of that vision because it is the only location in the Masterplan where the highest building can be positioned. The demolition of 32-38 Station Road is necessary and justified for this reason. This point of view is now more rigorously supported by information submitted by the applicants which I have addressed above.

8.14 In reaching this view I am mindful of the Senior Conservation Officer's view as follows:

'The loss of BLIs in a prominent location in the CA is always a matter for regret and the policies at national & local level are against such a loss except in cases where there is a demonstrable benefit to be derived. In this case, the decision makers who granted outline permission for the 'masterplan' were aware of the presumed demolition and assessed it to be worthwhile. All the discussions about the replacement building were predicated on the design having to be assessed as of suitable quality to meet the policy 'tests'.'

8.15 I share the Senior Conservation Officer's view that the design of the new building is an extremely important consideration. All of the discussions with the applicants have been based on the need for the new building to be appropriate for its setting in the Conservation Area.

8.16 I am convinced that the new office buildings will be a positive asset to the Conservation Area for the reasons set out in my report on the planning application. The Design and

Conservation Panel, English Heritage and the Urban Design and Conservation Team share these views. These views provide the necessary justification for the demolition of 32-38 Station Road. In particular I note the views of English Heritage which supports the adoption of a 'holistic view' to redevelopment in this area and that of the Design and Conservation CB1 Sub-Panel which reached the unanimous conclusion that the revised design for the office building is an improvement over the previous scheme.

- 8.17 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 4/11 and 4/12 and guidance provided by the NPPF and overcomes the reasons for refusal of the previous application.

Other environmental impacts

- 8.18 I have recommended conditions and informatives as recommended by the Highway Authority officer and the Environmental Health Officer.
- 8.19 In my opinion, subject to compliance with conditions, the proposal is compliant with Cambridge Local Plan (2006) policies 4/13 and 8/2.

Third Party Representations

- 8.20 I have addressed the issues raised in this report and in the related report on the planning application. I do not consider employment generation to be a relevant consideration to this application.

9.0 CONCLUSION

- 9.1 In my view the demolition of 32-38 Station Road is justified by the public benefit that will derive from the development of the site for office accommodation as part of the wider Station Area redevelopment. The assessment of 32-38 Station Road as a BLI and heritage asset has been carried out properly and is supported by officers. I have no objections to the demolition which will facilitate the delivery of the CB1 Masterplan approved under the Outline Planning Consent.

10.0 RECOMMENDATION

APPROVE subject to the following conditions and reasons for approval:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In accordance with the requirements of section 51 of the Planning and Compulsory Purchase Act 2004.

2. No development shall take place until a full photographic record and survey by measured drawing and salvage of samples has been made depicting the exterior and interior of the building (including any parts to be demolished) and a copy deposited with each of the following organisations: the Cambridgeshire Collection of the Central Library, Lion Yard, Cambridge; the County Archive, Shire Hall, Castle Hill, Cambridge, and the local planning authority. The precise number and nature of the photographs, drawings and samples to be taken is to be agreed in advance with the local planning authority and the format in which they are to be displayed and titled is to be agreed with the local planning authority before the deposit is made.

Reason: To foster understanding of the building's importance in the national and Cambridge context, and to ensure proper recording of any aspects of the building's special interest which are to be lost or altered. (Cambridgeshire and Peterborough Structure Plan 2003, policy P7/6 and Cambridge Local Plan 2006, policy 4/12)

3. Prior to the commencement of the development hereby permitted, including any demolition or enabling works, a detailed Construction Method Statement (CMS) shall be submitted to and approved by the local planning authority in writing.

The Construction Method Statement shall be accompanied by:

1. A statement that demonstrates how the proposal accords with the approved Construction Environmental Management Plan (CEMP), Eight Issue dated 5 January 2011 (condition 31 of the outline planning permission reference 08/0266/OUT).

In addition the CMS shall also provide:

- 2 A specific construction programme
- 3 A plan identifying:
 - (a) The contractor site storage area/compound
 - (b) Screening and hoarding locations
 - (c) Access arrangements for:
 - i. Vehicles
 - ii. Plant
 - iii. Personnel
 - iv. Building material
 - (d) Plant and equipment storage areas
 - (e) Contractor parking arrangements for:
 - i. Construction vehicles
 - ii. Personnel vehicles
 - (f) The location of contractor offices

Thereafter the development shall be undertaken in accordance with the agreed details unless otherwise agreed in writing by the local planning authority.

Reason: To ensure the environmental impact of the construction of the development is adequately mitigated and in the interests of the amenity of nearby residents/occupiers (Cambridge Local Plan 2006 policy 4/13).

INFORMATIVE: Applicants or their agents preparing a survey for archive deposit are advised to refer to "Understanding Historic Buildings: A guide to good recording practice." - English Heritage, 2006.

INFORMATIVE: New development can sometimes cause inconvenience, disturbance and disruption to local residents, businesses and passers by. As a result the City Council runs a Considerate Contractor Scheme aimed at promoting high standards of care during construction. The City Council encourages the developer of the site, through its building contractor, to join the scheme and agree to comply with the model Code of Good Practice, in the interests of good neighbourliness. Information about the scheme can be obtained from The Considerate Contractor Project Officer in the Planning Department (Tel: 01223 457121).

This development involves work to the public highway that will require the approval of the County Council as Highway Authority. It is an OFFENCE to carry out any works within the public highway, which includes a public right of way, without the permission of the Highway Authority. Please note that it is the applicant's responsibility to ensure that, in addition to planning permission, any necessary consents or approvals under the Highways Act 1980 and the New Roads and Street Works Act 1991 are also obtained from the County Council. Public Utility apparatus may be affected by this proposal. Contact the appropriate utility service to reach agreement on any necessary alterations, the cost of which must be borne by the applicant.

Reasons for Approval

1. This development has been approved, conditionally, because subject to those requirements it is considered to conform to the Development Plan as a whole, particularly the following policies:

East of England plan 2008: ENV7

Cambridge Local Plan (2006): 4/11 4/12 4/13 8/2 9/9

2. The decision has been made having had regard to all other material planning considerations, none of which was considered to have been of such significance as to justify doing other than grant planning permission.

These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.

**Cambridge City Council
Design & Conservation (CB1) Sub-Panel**

Notes of the meeting Wednesday 14th March 2012

Present:

Dr Nick Bullock	Chair
Terry Gilbert	RTPI (vice Chair)
Richard Owers	RIBA
Carolyn Gohler	Cambridge Past, Present & Future
Jon Harris	Co-opted Member
David Grech	English Heritage

Officers:

Glen Richardson	City Council
Sarah Dyer	City Council
Jonathan Brookes	City Council

Presenters:

Neven Sidor	Grimshaw Architects
Eric Osborne	Grimshaw Architects
Robert Myers	Robert Myers Associates

Observers:

Sven Topel	Brookgate Developments
Derek Ford	Brookgate Developments
Jon Burgess	Beacon Planning

1. Apologies – Oliver Caroe

2. Introduction to 50/60 Station Road by Glen Richardson.

A note prepared by Glen Richardson explaining the background to today's presentation had been circulated in advance.

A proposal for this site was last seen by the Panel in November 2011 (verdict AMBER). City Council officers working on the scheme throughout last year expressed significant concerns on issues such as its compliance with the approved CB1 parameter plans (principally the requirements for the I2 block), whether the buildings were a matched pair, the overall height of the building(s) and the approach to fenestration, materials and renewables, amongst others. The application was submitted without, in effect, these matters being fully resolved and was then reviewed by the sub-panel at the November meeting.

In early December 2011, officers gave a clear steer to the applicant that the submitted application could not be supported on design grounds. The architects then took the officer comments and over a series of meetings and design iterations have produced a scheme which, in officer opinion at least, is better resolved overall and which is improved in respect of scale, mass, materials and composition.

The lead architect, Neven Sidor, described the features of the amended proposal. These included the following:

- o Differentiation between the different sides of the buildings.
- o A landscape scheme adapted to the new architectural layout.
- o The facades now seen as an expression of light, not mass, as demonstrated by detailed modelling.
- o A design without louvers this time around, and instead with reconstituted stone fins and of a more solid nature
- o A glazed entrance lobby
- o A view through to the station building down the Southern Access Road.
- o A more generous colonnade facing Station Road.
- o A public artwork setting the tone for Station Road.

The Panel's comments are as follows:

- ☐ The Panel welcomed the new design strategy for the development with its revised massing and the clear differentiation of the two buildings.
- ☐ The Station Road frontage. The Panel welcome the double height of the ground floor of the two towers and the more generous approach to the design of this area. This and the redesign of the 'Pod' is likely to generate more activity along this frontage could, with the proposed cycle racks, lead to greater conflict between cyclists and pedestrians. The Panel expressed some scepticism about the adequate management of the cycle parking and the ability of the developer to prevent its use by rail travellers.
- ☐ The South Square frontage. The Panel welcomed the chamfering of the south-eastern corner of No 60 at street level and the view through to the Station Square and the level of animation at the northern edge of the South Square that this will make possible.
- ☐ The Café area, the Station Road frontage. The Panel questioned whether the area would be as open as it appeared from the presentation and asked that the dimensions of this space be checked. While the Panel generally favoured the openness of the

area around the café, there was concern about the possible crowding of activity with the café, pedestrians en route to the station and cyclists leaving their bikes.

- ☐ The 'Pod'. The Panel welcome the architectural language and animation of this single height space set against the double height of the two foyers.
- ☐ The Stair cores, No 60 and 50. The Panel favoured the differentiation and the placing of the two cores and the way that the core to No 60 would clearly signal the position of the development on Station Road.
- ☐ West elevation of No 50. The core does not continue to ground level and the Panel thought that the choice of stone or precast units for the elevation of the 'base' element of the building, particularly around the junction of the western and the Station Road elevations, needed further consideration. The Panel also raised the issue of the detailed topography of the area and the need to consider carefully the relative levels in the handling of the ground floor of the development.
- ☐ Rear stair core No 60. There is no visible expression of the stair tower, and the members of the Panel questioned this arrangement, in particular the treatment of the stair core especially at ground floor level.
- ☐ Details of the cladding of the frame, the 'fins' and the base elements. The Panel are confident that the proposed 'kit of parts' will provide the basis for a successful treatment of the elevations but feel that further refinement of the proposals for both sets of elevations is necessary to recognise, for example, the different conditions for the northern and the southern elevations.
- ☐ The choice of the 'family' of materials. In general terms, the Panel favours the development of an architectural language with a restrained palette of materials.
- ☐ The detail design of the 'fins'. The Panel raised the dangers of weather staining on the 'fins' and looks forward to seeing the details of the design that will address this issue.
- ☐ Reconstituted stone elements. Although the Panel recognise the reasons behind this choice of material, careful control of the finishing, handling and installation of these elements will be needed to avoid the kind of chipping seen on other developments in the City.
- ☐ The Panel would welcome an approach to detailing that would take account of the approach adopted for neighbouring buildings.
- ☐ Green roof over cycle parking (No 50). This is welcomed. Opportunities for green roofs above other parts of the building should also be explored.

- ☐ Tree guards/shelter. The Panel consider that tree guards must be made to a robust design if they are to succeed.
- ☐ Planting (in the space between buildings) facing south. The Panel are confident that this arrangement would be successful, but recommend some protective measures being taken against a possible wind “vortex”, perhaps through the inclusion of a taller glazed element at the southern end of the open terrace.
- ☐ The panel note the scheme will have a BREEAM Excellent rating.
- ☐ Public art. The Panel welcome the suggestion that the stair tower of No 60 might be used for public art. The form that this might take needs to be determined in consultation with the City’s Public Art Panel.

Conclusion

In strategic terms, the Panel considers that the new approach is a great improvement. The change in massing, the handling of the frontage at ground level along Station Road and the greater animation of the frontage to the ‘anti-chamber’ square to the south are welcomed. The ‘kit of parts’ proposed for the elevations looks promising but further refinement of the design is still needed, as is further examination of the treatment of stair cores at ground level.

VERDICT –

1. The strategy of the massing, the relationship of the stair core with Station Road, the overall strategy for the elevations and the handling of the public realm, GREEN (5), AMBER (1)

2. The ‘kit of parts’ for the elevations, the handling of the elevations at ground level, the design of the ‘fins’ and other components and the planting of the terraced area, GREEN (3), AMBER (2)

Reminder:

CABE ‘traffic light’ definitions:

GREEN: a good scheme, or one that is acceptable subject to minor improvements

AMBER: in need of *significant* improvements to make it acceptable, but not a matter of starting from scratch

RED: the scheme is fundamentally flawed and a fresh start is needed.



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50 & 60 STATION ROAD

STATION

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50 & 60 STATION ROAD

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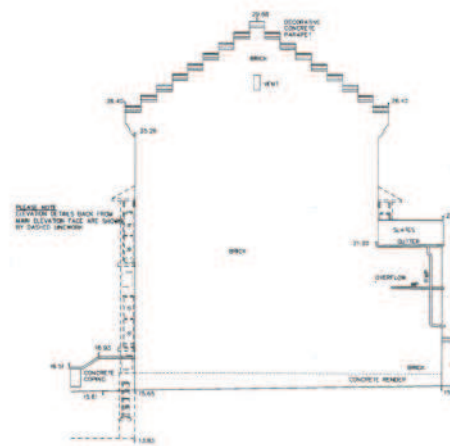
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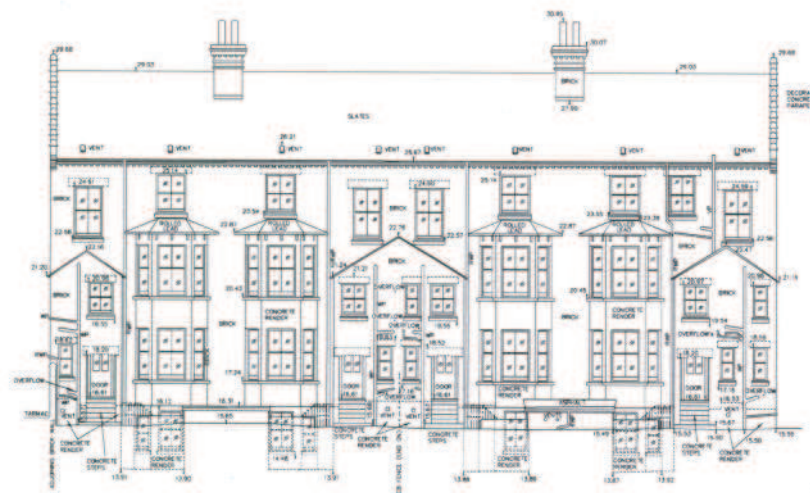
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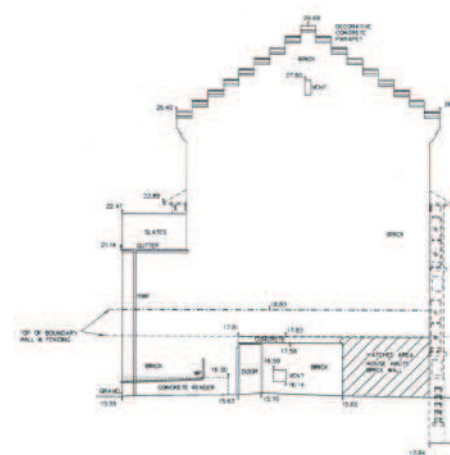
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WEST ELEVATION



SOUTH ELEVATION



EAST ELEVATION

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Application Number	12/1512/FUL	Agenda Item	
Date Received	23rd November 2012	Officer	Mr John Evans
Target Date	18th January 2013		
Ward	Castle		
Site	1 Benson Place Cambridge CB4 3QP		
Proposal	Erection of 8 x 1 bed dwellings with associated cycle parking, hard and soft landscaping and refuse storage, following the demolition of the existing bungalow and garage.		
Applicant	c/o Agent		

SUMMARY	<p>The development accords with the Development Plan for the following reasons:</p> <ol style="list-style-type: none"> 1. The proposed development will not adversely affect the open character of the street scene in the Conservation Area. 2. The detailed design of the building will make a positive contribution to the character and appearance of the Conservation Area. 3. No significant adverse harm to the amenities of properties along Huntingdon Road or the health of nearby trees.
RECOMMENDATION	APPROVAL

1.0 SITE DESCRIPTION/AREA CONTEXT

- 1.1 The application site is a key shaped plot situated on the southern side of the junction of Benson Place and Westfield Lane. A bungalow with a rectangular shaped footprint and garage, covering approximately 50% of the plot, currently occupies the plot. The northern most part of the site is used as a private garden defined with a low fence. There is a communal

L shaped access to the south and west boundary of the site giving access to the rear gardens of numbers 18, 20 and 22 Huntingdon Road.

- 1.2 To the north west of the site is the Lutheran Church, a three storey building, formed of 2 octagon shaped wings constructed from grey concrete blocks. To the immediate north of the site is a relatively new building containing student accommodation, which has three levels of accommodation. To the east of the site are the rear gardens of numbers 23 to 27 Histon Road, where permission has been granted for three new dwelling houses. Beyond to the north east is a substantial rectangular shaped light industrial premises, currently used by Arbury Fast Fit.
- 1.3 The area is characterised by a tight urban grain, relatively narrow streets, with a mix of residential buildings. The area has a quiet, 'back lane' character, despite the presence of the commercial garage workshop along North Street.
- 1.4 The site falls within the Castle and Victoria Road extension of the Central Conservation Area. There are 3 mature trees in the vicinity of the site, protected from felling by reason of being within a Conservation Area.

2.0 THE PROPOSAL

- 2.1 Permission is sought for the erection of a new building containing eight, one bedroom apartments. The building is formed from two main sections. The rear section has three levels of accommodation with an eaves height of approximately 7m and a maximum ridge height of some 9m. The site levels rise to the south towards Huntingdon Road, giving an eaves height of 5.7m when measuring from the southern elevation.
- 2.2 The front section has two levels of accommodation with an eaves height of 4.5m and an overall ridge height of 6.7m. The northern most triangular section of the site is retained as landscaped garden.
- 2.3 The application is accompanied by the following supporting information:

1. Design and Access Statement

2. Planning Statement
3. Utilities Statement
4. Ecological Walkover Survey
5. Heritage Statement
6. Tree Survey

Amended Plans

Following the consultation responses the application has been amended with the following minor alterations. The alterations are not of such significance as to justify reconsultation of neighbouring residential properties.

- Revised landscape plan, showing the provision of a single feature tree in the triangular open space to the north of the site and low railings.
- Repositioning of the rear south facing bedroom window within the east elevation.
- Revised cycle parking arrangements.

The application is brought before Planning Committee because the neighbourhood consultation zone crosses the boundary between two wards that are covered by different Area Committees.

3.0 SITE HISTORY

Reference	Description	Outcome
12/1513/CAC	Demolition of existing dwelling and garage	Concurrent application

4.0 PUBLICITY

4.1	Advertisement:	Yes
	Adjoining Owners:	Yes
	Site Notice Displayed:	Yes

5.0 POLICY

- 5.1 See Appendix 1 for full details of Central Government Guidance, East of England Plan 2008 policies, Cambridgeshire and Peterborough Structure Plan 2003 policies, Cambridge

Local Plan 2006 policies, Supplementary Planning Documents and Material Considerations.

5.2 Relevant Development Plan policies

PLAN	POLICY NUMBER
Cambridgeshire and Peterborough Structure Plan 2003	P6/1 P9/8 P9/9
Cambridge Local Plan 2006	3/4 3/6 3/7 3/10 3/11 3/12 4/4 4/10 4/11 5/1 8/2 8/6 10/1

5.3 Relevant Central Government Guidance, Supplementary Planning Documents and Material Considerations

Central Government Guidance	National Planning Policy Framework March 2012 Circular 11/95 Community Infrastructure Levy Regulations 2010
Supplementary Planning Documents	Sustainable Design and Construction Planning Obligation Strategy
Material Considerations	<u>Central Government:</u> Letter from Secretary of State for Communities and Local Government (27 May 2010) Written Ministerial Statement: Planning for Growth (23 March 2011)

	Citywide: Open Space and Recreation Strategy
	<u>Area Guidelines:</u> Western Corridor Area Transport Plan <u>Conservation Area Appraisal:</u> Castle and Victoria Road

6.0 CONSULTATIONS

Arboriculture Officer

- 6.1 Further to assessment of the above I am satisfied that there are sufficient roots within the area to be developed to be a material consideration.

I am however satisfied that the use of a specially designed foundation that minimises the depth of ground beams/floor slabs within the RPA not already developed will help to minimise root damage.

I have no objection to the development subject to a suitable condition.

Cambridgeshire County Council (Transport)

- 6.2 The site is located close to the City Centre, with access to public transport, car club vehicles and cycle routes. The adjacent streets are subject to a Residents' parking Scheme and other on-street controls. There is, therefore, a significant deterrent to discourage residents from keeping a car and sufficient alternatives to make such an option practicable. Therefore the development would seem suitable for a car free scheme.

Second Consultation on S106 Obligations

We have reviewed this and in view of the additional information provided we can now confirm that a trip rate of 5.1 all mode

movements would be acceptable on this occasion for this development.

The trip rate calculation would therefore be:

$$8 \times 5.1 = 41 \text{ trips}$$

This falls below the 50 trip net increase in trips over which contributions towards the WCATP are sought. Therefore no contribution is required for this development on this occasion.

Head of Environmental Services

- 6.3 No objections, subject to construction hours related conditions.

Historic Environment Manager

- 6.4 Permitting the construction of the proposed building will dramatically change the street scene. However this will not necessarily be a negative change. Instead of the eye being drawn to the wide low view and down the rear access to houses on Histon Road, there is a newly created focal point. This will successfully preserve the character and appearance of the Conservation Area, in line with the Cambridge Local Plan Policy 4/11.

The success of the proposal will depend on a strong landscaping scheme to relate the building to its context. This should be dealt with through a condition to ensure that the landscaping is designed to complement the proposal.

The proposal would increase the level of development on the site and surrounding area. It is hoped that this will not set a precedent for future development as further building in the area could lead to over development, which would be harmful to the character and appearance of the Conservation Area.

Cambridgeshire County Council (Education)

- 6.5 Awaiting comments.

Cambridge City Council (Landscape)

- 6.6 Should this application be approved we will require the following: Fully detailed soft landscape proposals, to include

detailed planting plans, written specifications (including plant schedule with size, spacing and densities of proposed plants), and an implementation programme. With particular reference to the frontage boundary.

- 6.7 Fully detailed hard landscape proposals to include full construction details, levels, specifications of all hard surfacing materials, furniture, boundary treatments, lighting etc. A maintenance plan for the entire site (to include a 5-year replacement-planting regime at least).

Cambridge City Council Access Officer

- 6.8 Awaiting comments.
- 6.9 The above responses are a summary of the comments that have been received. Full details of the consultation responses can be inspected on the application file.

7.0 REPRESENTATIONS

- 7.1 Councillor Kightley has commented on this application. I have set out his comments below:

I have now been approached by a number of residents concerned about the proposed development in Benson Place (12/1512/FUL) and would like to see this come to committee if you are minded to approve.

*This is in the light of the refusal of a previous scheme and concerns about over-development *per se* and the impact of the proposed development in a residents' parking zone specifically.*

- 7.2 The owners/occupiers of the following addresses have made representations:

7 Benson Place
8 Benson Place
10 Benson Place
14 Benson Street
18 Benson Street
7 High Street
Cranwell Court, Histon Road

7 Cranwell Court
14 Cranwell Court
12 Huntingdon Road
14 Huntingdon Road
16 Huntingdon Road
20 Huntingdon Road
24 Huntingdon Road
8 Canterbury Street
27 Histon Road

7.3 The representations can be summarised as follows:

Objections in principle

- The existing building is not very old and is not beyond renovation.
- Replacing viable homes is environmentally unsound.
- The proposal is not aimed at those who would be investing in the community.

Design issues

- Overdevelopment.
- Houses are not built up to the boundary line in the vicinity of the site.
- The surrounding streets are narrow and unsuited to high levels of traffic
- The design and character of the development is not in keeping.
- The proposed development would dominate Benson Place.
- The new building would dominate Benson Place and Westfield Lane.
- Proposed 'micro orchard' may become an eyesore.

Amenity

- Height will affect the privacy of number 22 Huntingdon Road.

- Additional noise impact.
- There is no right of way to use the rear access.
- The re-sited telegraph pole is unacceptable.
- The balconies will cause overlooking problems.
- Overlooking of gardens of numbers 16, 18, 20 and 22 Huntingdon Road.
- Damage to trees.

Car parking concerns

- Inadequate car parking.
- Impact of construction traffic.

Other matters

- Damage to drainage.

7.3 The above representations are a summary of the comments that have been received. Full details of the representations can be inspected on the application file.

8.0 ASSESSMENT

8.1 From the consultation responses and representations received and from my inspection of the site and the surroundings, I consider that the main issues are:

1. Principle of development
2. Context of site, design and external spaces
3. Disabled access
4. Residential amenity
5. Refuse arrangements
6. Highway safety
7. Car and cycle parking
8. Third party representations
9. Planning Obligation Strategy

Principle of Development

- 8.2 The provision of additional dwellings on previously developed land, and the provision of higher density housing in sustainable locations is generally supported by central government advice contained within the National Planning Policy Framework 2012. Policy 5/1 of the Cambridge Local Plan 2006 allows for residential development from windfall sites, subject to the existing land use and compatibility with adjoining uses, which is discussed in more detail in the amenity section below. The proposal is therefore in compliance with these policy objectives.
- 8.3 The NPPF declassifies garden land from the definition of brownfield land and such sites are no longer included within the Authority's five year housing land supply. This notwithstanding, Local Plan policy 3/10 sets out the relevant criteria for assessing proposals involving the subdivision of existing plots which remain acceptable in principle, subject to design and the impact on the open character of the area. Policy 3/10 recognises the important part of the character and amenity value gardens contribute to the City.
- 8.4 In principle, policy 3/10, allows for proposals involving the subdivision of existing plots in the garden area or curtilage of existing dwellings. Development of this nature will not be permitted however if it will have a significant adverse impact on the amenities of neighbouring properties through loss of privacy, light or an overbearing sense of enclosure; provide inadequate amenity space, or detract from the prevailing character and appearance of the area. An analysis of these issues is provided in the design and amenity sub sections below.
- 8.5 The development involves the demolition of the existing bungalow, whereby the criteria of Local Plan policy 4/10 are relevant. The bungalow is a neutral building in the Conservation Area. While not structurally unsound, in my view the wider public benefits from redevelopment satisfy part c of policy 4/10.
- 8.6 There is no objection in broad principle to residential development, but the proposal has to be assessed against the criteria of other relevant development plan policies. In my opinion, the development of the plot in this manner will not have

a significant harmful impact on the open character of the street scene. The principle of subdivision is therefore acceptable and in accordance with Cambridge Local Plan 2006 policies 3/10 and 5/1.

Context of site, design and external spaces

- 8.7 The key design issue is the design and appearance of the new building within the Conservation Area.
- 8.8 The site is an anomaly because of its unusual dimensions; redevelopment has the potential to make a positive contribution to the character and appearance of the street scene. The application site has a prominent backdrop when viewed south east from Westfield Lane and south from North Street. In my opinion the siting of the building will positively frame the corner of the street. Some concerns have been raised regarding the siting of the building abutting the pavement edge along Benson Place. There are a range of building types in the vicinity, some of which, for example numbers 2 and 2 b Benson Street, and The Lutheran Church, also abut the pavement edge in this manner. Given the unusual shallow width of the plot, I do not consider the siting of the building to detract from the character of the street scene. The footprint of the proposed building positively responds to the site constraints and is compliant with Cambridge Local Plan policy 3/12.
- 8.9 The southern most section of the proposed building has three levels of accommodation. I note some concerns regarding its overall height. The stepped increase in height to the south is logical because of the rise in land levels to the substantial terraces along Huntingdon Road. The proposed building would not in my view appear overly intrusive or visually prominent when viewed south from North Street or Westfield Lane. The overall scale is consistent with the student accommodation opposite and the Lutheran Church. The proposed mix of mono pitched roofs, and shallow footprint will ensure that the scale and mass of the new street frontage is harmonious in its surroundings.
- 8.10 External spaces have been subdivided in a logical manner providing useable private courtyards. The footprint of the new building does not transgress into the triangular open space to

the north of the site, which is currently open garden. This space contributes to views across North Street and Benson Place and the overall composition and character of the space. It will remain as open garden, which in my view is a positive design response. The detailed landscaping of this space can be agreed through the imposition of suitable planning conditions.

- 8.11 The proposed materials of construction, a mix of buff brickwork, slate roofing, larch cladding and zinc detailing, will complement the character and appearance of the Conservation Area and create a visually attractive new street scene.

Trees

- 8.12 There are three mature trees in the vicinity of the site, the closest, a Sycamore is sited to the immediate south of the site, in the garden of 18 Huntingdon Road. This tree has been identified as having the root protection area within the proposed development. The Arboriculture Officer requested a report to ascertain the possible impact of the proposed development as a result of below ground conflict. Some roots were found within the Root Protection Area, but the Arboriculture Officer is satisfied that the use of specifically designed foundations will minimise the depth of ground beams. Consequently, the proximity of the tree should not constrain development. The Arboriculture officer is satisfied that potential root damage can be minimised, subject to the imposition of a suitable planning condition. (Condition 11)
- 8.13 In my opinion the proposal is an acceptable plot subdivision, which will not detract from the open character of the Conservation Area. The proposal is compliant with Cambridge Local Plan (2006) policies 3/4, 3/7, 3/10, 3/12, 4/4 and 4/11.

Residential Amenity

Impact on amenity of neighbouring occupiers

- 8.14 The proposed new building is sited between 25m and 30m from the rear of numbers 16 to 22 Huntingdon Road. Given the distances involved I do not consider the building to create an unneighbourly visual impact. Given the northerly location of the

application site, there will be no overshadowing cast onto the end section of the long rectangular rear gardens of the Huntingdon Road properties. The relatively higher ground of the rear gardens of Huntingdon Road, and existing tree cover, will also reduce the perceived visual intrusion from the new building. The proposed south facing small bedroom window has now been repositioned to face east to eliminate the possibility of overlooking into the gardens to the south.

- 8.15 The development will not create any overlooking or visual intrusion to the new residential dwellings to be constructed to the east. The proposed Juliet balconies are logically positioned to provide surveillance of the street. In my opinion the proposal adequately respects the residential amenity of its neighbours and the constraints of the site and I consider that it is compliant with Cambridge Local Plan (2006) policies 3/4 and 3/7.

Amenity for future occupiers of the site

- 8.16 The development provides desirable accommodation, and given the size and nature of the units, is very well served by the creative integration of private amenity spaces. The ground floor units benefit from 3 private courtyards. The upper floor units are served with small balcony areas, with flat S2 benefiting from a secluded terrace area.
- 8.17 In my opinion the proposal provides a high-quality living environment and an appropriate standard of residential amenity for future occupiers, and I consider that in this respect it is compliant with Cambridge Local Plan (2006) policies 3/7 and 3/12.

Refuse Arrangements

- 8.18 Refuse storage has been adequately integrated into the scheme in the form of a communal store. In my opinion the proposal is compliant with Cambridge Local Plan (2006) policy 3/12.

Car and Cycle Parking

- 8.19 The development is very well served by cycle parking providing a total of 12 spaces which exceeds the Council's minimum standard of 8 spaces. The three ground floor flats benefit from

their own private secure provision within the private courtyards. Four of the upper floor flats have access to a secure enclosure as set out within the revised plan C040/134. The fifth upper floor flat has parking provision close to the main entrance under cover. Two visitor spaces are provided.

- 8.20 The development is car free. The site is within the controlled parking zone, so there will be no additional pressure for on-street spaces. The site is also very well serviced by public transport, cycling routes and car club options. There are viable alternatives to the private car. In my opinion, the proposal is compliant with Cambridge Local Plan (2006) policies 8/6 and 8/10.

Third Party Representations

- 8.21 The comments received have been covered in the above report. The following issues have also been raised:

There is no right of way to use the rear access.

This is a legal matter and not material to the determination of this application. The applicant is satisfied future occupants would have use of the access.

The re-sited telegraph pole is unacceptable

The positioning of telecoms infrastructure is a matter for the statutory operator providing the service. The erection of telegraph poles is 'permitted development'.

Damage to drainage

The applicant has submitted a utilities assessment. The development will integrate with the existing drainage infrastructure. Separate consent may be required from Anglia Water.

Impact of construction traffic

This can be managed through the imposition of suitable planning conditions.

Planning Obligation Strategy

8.22 The Community Infrastructure Levy Regulations 2010 have introduced the requirement for all local authorities to make an assessment of any planning obligation in relation to three tests. If the planning obligation does not pass the tests then it is unlawful. The tests are that the planning obligation must be:

- (a) necessary to make the development acceptable in planning terms;
- (b) directly related to the development; and
- (c) fairly and reasonably related in scale and kind to the development.

The proposed development triggers the requirement for the following community infrastructure:

Open Space

8.23 The Planning Obligation Strategy requires that all new residential developments contribute to the provision or improvement of public open space, either through provision on site as part of the development or through a financial contribution for use across the city. The proposed development requires a contribution to be made towards open space, comprising outdoor sports facilities, indoor sports facilities, informal open space and provision for children and teenagers. The total contribution sought has been calculated as follows.

(Net increase of 6 bedrooms).

Outdoor sports facilities					
Type of unit	Persons per unit	£ per person	£per unit	Number of such units	Total £
studio	1	238	238		
1 bed	1.5	238	357	6	2142
2-bed	2	238	476		
3-bed	3	238	714		
4-bed	4	238	952		
Total					2142

Indoor sports facilities					
Type of unit	Persons per unit	£ per person	£per unit	Number of such units	Total £
studio	1	269	269		
1 bed	1.5	269	403.50	6	2421
2-bed	2	269	538		
3-bed	3	269	807		
4-bed	4	269	1076		
Total					2421

Informal open space					
Type of unit	Persons per unit	£ per person	£per unit	Number of such units	Total £
studio	1	242	242		
1 bed	1.5	242	363	6	2178
2-bed	2	242	484		
3-bed	3	242	726		
4-bed	4	242	968		
Total					2178

8.24 Subject to the completion of a S106 planning obligation to secure the requirements of the Planning Obligation Strategy (2010) and the Cambridge City Council Open Space Standards Guidance for Interpretation and Implementation (2010), I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and P9/8, Cambridge Local Plan (2006) policies 3/8 and 10/1 and the Planning Obligation Strategy 2010 and the Cambridge City Council Open Space Standards Guidance for Interpretation and Implementation (2010)

Community Development

8.25 The Planning Obligation Strategy (2010) requires that all new residential developments contribute to community development facilities, programmes and projects. This contribution is £1256 for each unit of one or two bedrooms and £1882 for each larger

unit. The total contribution sought has been calculated as follows:

Community facilities			
Type of unit	£per unit	Number of such units	Total £
1 bed	1256		
2-bed	1256	7	8792
3-bed	1882		
4-bed	1882		
Total			8792

- 8.26 Subject to the completion of a S106 planning obligation to secure the requirements of the Planning Obligation Strategy (2010), I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and P9/8, Cambridge Local Plan (2006) policies 5/14 and 10/1 and the Planning Obligation Strategy 2010.

Waste

- 8.27 The Planning Obligation Strategy (2010) requires that all new residential developments contribute to the provision of household waste and recycling receptacles on a per dwelling basis. As the type of waste and recycling containers provided by the City Council for houses are different from those for flats, this contribution is £75 for each house and £150 for each flat. The total contribution sought has been calculated as follows:

Waste and recycling containers			
Type of unit	£per unit	Number of such units	Total £
House	75		
Flat	150	7	1050
Total			1050

- 8.28 Subject to the completion of a S106 planning obligation to secure the requirements of the Planning Obligation Strategy (2010), I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and P9/8, Cambridge Local Plan (2006) policies 3/7, 3/12 and 10/1 and the Planning Obligation Strategy 2010.

Waste Management

- 8.29 A contribution is sought from all dwellings towards up grading existing/providing new Household Recycling Centres to mitigate the impact of new development on these facilities. This development lies within the catchment site for Milton. Contributions are sought on the basis of £190 per house for four new sites giving increased capacity as permanent replacements for the existing temporary site at Milton. A total contribution of £1330 is necessary.
- 8.30 Subject to the completion of a S106 planning obligation to secure the requirements of the RECAP Waste Management Design Guide SPD 2012, I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and P9/8, Cambridge Local Plan (2006) policy 10/1 and the RECAP Waste Management Design Guide SPD 2012.

Education

- 8.31 Upon adoption of the Planning Obligation Strategy (2010) the Council resolved that the Education section in the 2004 Planning Obligations Strategy continues to apply until it is replaced by a revised section that will form part of the Planning Obligations Strategy 2010. It forms an annex to the Planning Obligations Strategy (2010) and is a formal part of that document. Commuted payments are required towards education facilities where four or more additional residential units are created and where it has been established that there is insufficient capacity to meet demands for educational facilities. Contributions are therefore required on the following basis.

Life-long learning					
Type of unit	Persons per unit		£per unit	Number of such units	Total £
1 bed	1.5		160		1120
2+- beds	2		160		
Total					1120

- 8.32 Subject to the completion of a S106 planning obligation to secure the requirements of the Planning Obligation Strategy 2010, I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and P9/8, Cambridge Local Plan (2006) policies 5/14 and 10/1 and the Planning Obligation Strategy 2010.

Transport

- 8.33 The County Highways Authority have confirmed that a trip rate of 5.1 all mode movements would be consistent with that applied for similar developments of this nature and location.

The 41 trips generated falls below the 50 trip net increase over which contributions towards the WCATP are sought. Therefore no contribution is required for this development.

Monitoring

- 8.34 The Planning Obligation Strategy (2010) requires that all new residential developments contribute to the costs of monitoring the implementation of planning obligations. The costs are calculated according to the heads of terms in the agreement. The contribution sought will be calculated as £150 per financial head of term, £300 per non-financial head of term. Contributions are therefore required on that basis.

Planning Obligations Conclusion

- 8.35 It is my view that the planning obligation is necessary, directly related to the development and fairly and reasonably in scale and kind to the development and therefore the Planning Obligation passes the tests set by the Community Infrastructure Levy Regulations 2010.

9.0 CONCLUSION

- 9.1 The proposed new building will not detract from the open character of the Conservation Area or the amenities of neighbours. APPROVAL is recommended.

10.0 RECOMMENDATION

1. APPROVE subject to the satisfactory completion of the s106 agreement by 1 May 2013 and subject to the following conditions and reasons for approval:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In accordance with the requirements of section 51 of the Planning and Compulsory Purchase Act 2004.

2. No development shall take place until samples of the materials to be used in the construction of the external surfaces of the development hereby permitted have been submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 3/14)

3. Prior to the commencement of development, large scale drawings of details of new sills, lintels, jambs, transoms, mullions, thresholds, shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details.

Reason: In the interests of the visual amenity of the Conservation Area. (Cambridge Local Plan 2006 policies 3/12 and 4/11).

4. A hard and soft landscaping scheme, including full details of surface and boundary treatments, is to be submitted to and approved in writing by the Local Planning Authority. Landscape works shall thereafter be constructed only in accordance with the approved details.

Reason: To avoid harm to the special interest of the Listed Building (Cambridge Local Plan 2006, policy 4/10).

5. No development shall take place until a schedule of landscape maintenance for a minimum period of five years has been submitted to and approved in writing by the local planning authority. The schedule shall include details of the arrangements for its implementation.

Reason: To ensure that the landscaped areas are maintained in a healthy condition in the interests of visual amenity. (Cambridge Local Plan 2006 policies 3/4, 3/11 and 3/12)

6. Except with the prior written agreement of the local planning authority no construction work or demolition shall be carried out or plant operated other than between the following hours: 0800 hours to 1800 hours Monday to Friday, 0800 hours to 1300 hours on Saturday and at no time on Sundays, Bank or Public Holidays.

Reason: To protect the amenity of the adjoining properties. (Cambridge Local Plan 2006 policy 4/13)

7. No development shall commence until a programme of measures to minimise the spread of airborne dust from the site during the demolition / construction period has been submitted to and approved in writing by the Local Planning Authority. The development shall be implemented in accordance with the approved scheme.

Reason: In the interests of neighbouring amenity, Cambridge Local Plan 2006 policy 4/13.

8. Prior to the commencement of the development hereby approved (including any pre-construction, demolition, enabling works or piling), the applicant shall submit a report in writing, regarding the demolition / construction noise and vibration impact associated with this development, for approval by the local authority. The report shall be in accordance with the provisions of BS 5228:2009 Code of Practice for noise and vibration control on construction and open sites and include full details of any piling and mitigation measures to be taken to protect local residents from noise and or vibration. Development shall be carried out in accordance with the approved details.

Reason: In the interests of neighbouring amenity, Cambridge Local Plan 2006 policy 4/13.

9. Except with the prior agreement of the local planning authority in writing, there should be no collection or deliveries to the site during the demolition and construction stages outside the hours of 0700 hrs and 1900 hrs on Monday ' Saturday and there should be no collections or deliveries on Sundays or Bank and public holidays.

Reason: In the interests of neighbouring amenity, Cambridge Local Plan 2006 policy 4/13.

10. Prior to the commencement of development/construction, a noise insulation scheme detailing the acoustic noise insulation performance specification of the external building envelope of the residential units (having regard to the building fabric, glazing and ventilation) to reduce the level of noise experienced in the residential units as a result of the proximity of the habitable rooms to the high ambient noise levels in the area (Histon Road façade dominated by traffic/commercial/plant/vehicle noise), be submitted to and approved in writing by the local planning authority. The scheme shall achieve the internal noise levels recommended in British Standard 8233:1999 'Sound Insulation and noise reduction for buildings-Code of Practice'. The scheme as approved shall be fully implemented before the use hereby permitted is commenced and shall not be altered without prior approval.

Reason: In the interests of the amenities of future occupiers, Cambridge Local Plan 2006 policy 4/13.

11. Details of the specification of any measures to be taken for the protection of any trees from damage during the course of development, shall be submitted to the local planning authority for its written approval, and implemented in accordance with that approval before any equipment, machinery or materials are brought onto the site for the purpose of development (including demolition). The agreed means of protection shall be adopted and adhered to throughout development until all equipment, and surplus materials have been removed from the site. Nothing shall be stored or placed in any area protected in accordance with this condition, and the ground levels within those areas shall not be altered nor shall any excavation be made without the prior written approval of the local planning authority.

Reasons: In the interests of providing adequate provision for the retention of trees, Cambridge Local Plan 2006 policy 4/4.

INFORMATIVE: New development can sometimes cause inconvenience, disturbance and disruption to local residents, businesses and passers by. As a result the City Council runs a Considerate Contractor Scheme aimed at promoting high standards of care during construction. The City Council encourages the developer of the site, through its building contractor, to join the scheme and agree to comply with the model Code of Good Practice, in the interests of good neighbourliness. Information about the scheme can be obtained from The Considerate Contractor Project Officer in the Planning Department (Tel: 01223 457121).

Reasons for Approval

1. This development has been approved, conditionally, because subject to those requirements it is considered to conform to the Development Plan as a whole, particularly the following policies:

Cambridge Local Plan (2006): 3/4, 3/6, 3/7, 3/10, 3/11, 3/12, 4/4, 4/10, 4/11, 5/1, 8/2, 8/6, 10/1.

2. The decision has been made having had regard to all other material planning considerations, none of which was considered to have been of such significance as to justify doing other than grant planning permission.

3. In reaching this decision the local planning authority has acted on guidance provided by the National Planning Policy Framework, specifically paragraphs 186 and 187. The local planning authority has worked proactively with the applicant to bring forward a high quality development that will improve the economic, social and environmental conditions of the area.

These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.

2. Unless prior agreement has been obtained from the Head of Planning, in consultation with the Chair and Spokesperson of this Committee to extend the period for completion of the Planning Obligation required in connection with this development, if the Obligation has not been completed by 1 March 2013, or if Committee determine that the application be refused against officer recommendation of approval, it is recommended that the application be refused for the following reason(s):

The proposed development does not make appropriate provision for public open space, community development facilities, life-long learning facilities, transport mitigation measures, waste facilities, waste management and monitoring in accordance with Cambridge Local Plan 2006 policies 3/8, 3/12 and 10/1 and as detailed in the Planning Obligation Strategy 2010, the Open Space Standards Guidance for Interpretation and Implementation 2010, the Southern Corridor Area Transport Plan 2002, the Western Corridor Area Transport Plan 2003 and the Cambridgeshire and Peterborough Waste Partnership (RECAP): Waste Management Design Guide Supplementary Planning Document 2012.

3. In the event that the application is refused, and an Appeal is lodged against the decision to refuse this application, delegated authority is sought to allow officers to negotiate and complete the Planning Obligation required in connection with this development

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WESTFIELD LANE



WEST ELEVATION



APPROVED SCHEMES (NOT BUILT)

Scale 0m 1m 2m 3m 4m 5m 6m 7m 8m 9m 10m

Project Name
PRELIMINARY ISSUE FOR COMMENT

CMP
Autodesk

Client
UNITE

Project
1. Brown Place
Leeds

Proposed Street Elevation

Date	By	For
1/10/2011	UNITE	28-05-2011

040 / 125

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Application Number	12/1513/CAC	Agenda Item	
Date Received	23rd November 2012	Officer	Mr John Evans
Target Date	18th January 2013		
Ward	Castle		
Site	1 Benson Place Cambridge CB4 3QP		
Proposal	Demolition of existing dwelling and garage		
Applicant	c/o Agent		

SUMMARY	<p>The development accords with the Development Plan for the following reasons:</p> <p>1. The demolition of the existing bungalow is justified by the wider benefits accruing from redevelopment.</p>
RECOMMENDATION	APPROVAL

1.0 SITE DESCRIPTION/AREA CONTEXT

- 1.1 The application site is a key shaped plot situated on the southern side of the junction of Benson Place and Westfield Lane. A bungalow with a rectangular shaped footprint and garage, covering approximately 50% of the plot, currently occupies the plot. The northern most part of the site is used as a private garden defined with a low fence. There is a communal L shaped access to the south and west boundary of the site giving access to the rear gardens of numbers 18, 20 and 22 Huntingdon Road.
- 1.2 To the north west of the site is the Lutheran Church, a three storey building, formed of 2 octagon shaped wings constructed from grey concrete blocks. To the immediate north of the site is a relatively new building containing student accommodation, which has three levels of accommodation. To the east of the site are the rear gardens of numbers 23 to 27 Histon Road, where permission has been granted for three new dwelling houses. Beyond to the north east is a substantial rectangular

shaped light industrial premises, currently used by Arbury Fast Fit.

- 1.3 The area is characterised by a tight urban grain, relatively narrow streets, with a mix of residential buildings. The area has a quiet, 'back lane' character, despite the presence of the commercial garage workshop along North Street.
- 1.4 The site falls within the Castle and Victoria Road extension of the Central Conservation Area. There are 3 mature trees in the vicinity of the site, protected from felling by reason of being within a Conservation Area.

2.0 THE PROPOSAL

- 2.1 Conservation Area Consent is sought for the demolition of the existing bungalow and garage outbuilding. The bungalow has a rectangular shaped footprint and is of brick construction with a mono pitched roof.
- 2.2 The Conservation Area Consent application has been submitted with the concurrent planning application for redevelopment of the site **12/1512/FUL**.

3.0 SITE HISTORY

Reference	Description	Outcome
12/1512/FUL	Erection of 8 x 1 bed dwellings with associated cycle parking, hard and soft landscaping and refuse storage, following the demolition of the existing bungalow and garage.	Concurrent application

4.0 PUBLICITY

- | | | |
|-----|------------------------|-----|
| 4.1 | Advertisement: | Yes |
| | Adjoining Owners: | Yes |
| | Site Notice Displayed: | Yes |

5.0 POLICY

5.1 See Appendix 1 for full details of Central Government Guidance, East of England Plan 2008 policies, Cambridgeshire and Peterborough Structure Plan 2003 policies, Cambridge Local Plan 2006 policies, Supplementary Planning Documents and Material Considerations.

5.2 Relevant Development Plan policies

PLAN	POLICY NUMBER
Cambridge Local Plan 2006	4/10 4/11

5.3 Relevant Central Government Guidance, Supplementary Planning Documents and Material Considerations

Central Government Guidance	National Planning Policy Framework March 2012 Circular 11/95 Community Infrastructure Levy Regulations 2010
Supplementary Planning Documents	Sustainable Design and Construction Planning Obligation Strategy
Material Considerations	<u>Central Government:</u> Letter from Secretary of State for Communities and Local Government (27 May 2010) Written Ministerial Statement: Planning for Growth (23 March 2011)
	Citywide: Open Space and Recreation Strategy

	<u>Area Guidelines:</u> Western Corridor Area Transport Plan <u>Conservation Area Appraisal:</u> Castle and Victoria Road
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6.0 CONSULTATIONS

Cambridgeshire County Council (Transport)

- 6.1 The site is located close to the City Centre, with access to public transport, car club vehicles and cycle routes. The adjacent streets are subject to a Residents' parking Scheme and other on-street controls. There is, therefore, a significant deterrent to discourage residents from keeping a car and sufficient alternatives to make such an option practicable. Therefore the development would seem suitable for a car free scheme.

Head of Environmental Services

- 6.2 No objections, subject to construction hours related conditions.

Historic Environment Manager

- 6.3 Permitting the construction of the proposed building will dramatically change the street scene. However this will not necessarily be a negative change. Instead of the eye being drawn to the wide low view and down the rear access to houses on Histon Road, there is a newly created focal point. This will successfully preserve the character and appearance of the Conservation Area, in line with the Cambridge Local Plan Policy 4/11.

The success of the proposal will depend on a strong landscaping scheme to relate the building to its context. This should be dealt with through a condition to ensure that the landscaping is designed to complement the proposal.

The proposal would increase the level of development on the site and surrounding area. It is hoped that this will not set a precedent for future development as further building in the area

could lead to over development, which would be harmful to the character and appearance of the Conservation Area.

Cambridgeshire County Council (Education)

- 6.4 Awaiting comments.

Cambridge City Council (Landscape)

- 6.5 Should this application be approved we will require the following: Fully detailed soft landscape proposals, to include detailed planting plans, written specifications (including plant schedule with size, spacing and densities of proposed plants), and an implementation programme. With particular reference to the frontage boundary.
- 6.6 Fully detailed hard landscape proposals to include full construction details, levels, specifications of all hard surfacing materials, furniture, boundary treatments, lighting etc. A maintenance plan for the entire site (to include a 5-year replacement-planting regime at least).

Cambridge City Council Access Officer

- 6.7 Awaiting comments.
- 6.8 The above responses are a summary of the comments that have been received. Full details of the consultation responses can be inspected on the application file.

7.0 REPRESENTATIONS

- 7.1 Councillor Kightley has commented on this application. I have set out his comments below:

I have now been approached by a number of residents concerned about the proposed development in Benson Place (12/1512/FUL) and would like to see this come to committee if you are minded to approve.

*This is in the light of the refusal of a previous scheme and concerns about over-development *per se* and the impact of the proposed development in a residents' parking zone specifically.*

7.2 The owners/occupiers of the following addresses have made representations:

7 Benson Place
8 Benson Place
10 Benson Place
18 Benson Street
7 High Street
Cranwell Court, Histon Road
12 Huntingdon Road
16 Huntingdon Road
20 Huntingdon Road
24 Huntingdon Road
8 Canterbury Street

7.3 The representations can be summarised as follows:

Objections in principle

- The existing building is not very old and is not beyond renovation.
- Replacing viable homes is environmentally unsound.
- The proposal is not aimed at those who would be investing in the community.

Design issues

- Overdevelopment.
- Houses are not built up to the boundary line in the vicinity of the site.
- The surrounding streets are narrow and unsuited to high levels of traffic
- The design and character of the development is not in keeping.
- The new building would dominate Benson Place and Westfield Lane.

Amenity

- Height will affect the privacy of number 22 Huntingdon Road.
- Additional noise impact.
- There is no right of way to use the rear access.
- The re-sited telegraph pole is unacceptable.
- The balconies will cause overlooking problems.
- Overlooking of gardens of numbers 16, 18, 20 and 22 Huntingdon Road.
- Damage to trees.

Car parking concerns

- Inadequate car parking.
- Impact of construction traffic.

Other matters

- Damage to drainage.

7.4 The above representations are a summary of the comments that have been received. Full details of the representations can be inspected on the application file.

8.0 ASSESSMENT

8.1 The key issue is whether the existing bungalow contributes to the character and appearance of the Conservation Area.

8.2 Local Plan policy 4/10 sets out the tests for proposals involving the demolition of buildings within a Conservation Area. The bungalow is a neutral building in the Conservation Area. It does not have any historic significance, or particular relationship to the unusual key shaped site which it occupies. While not structurally unsound, in my view the wider public benefits from redevelopment satisfy part c of policy 4/10.

- 8.3 Concerns raised by third parties relating to the redevelopment of the site are covered in the report for application 12/1512/FUL.

9.0 CONCLUSION

- 9.1 The demolition of the existing bungalow will not be harmful to the character and appearance of the Conservation Area. APPROVAL is recommended.

10.0 RECOMMENDATION

APPROVE subject to the following conditions:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In accordance with the requirements of section 51 of the Planning and Compulsory Purchase Act 2004.

2. Except with the prior written agreement of the local planning authority no construction work or demolition shall be carried out or plant operated other than between the following hours: 0800 hours to 1800 hours Monday to Friday, 0800 hours to 1300 hours on Saturday and at no time on Sundays, Bank or Public Holidays.

Reason: To protect the amenity of the adjoining properties. (Cambridge Local Plan 2006 policy 4/13)

3. Reasons for Approval

1. This development has been approved, conditionally, because subject to those requirements it is considered to conform to the Development Plan as a whole, particularly the following policies:

Cambridge Local Plan (2006): 4/10, 4/11.

2. The decision has been made having had regard to all other material planning considerations, none of which was considered to have been of such significance as to justify doing other than grant planning permission.

3. In reaching this decision the local planning authority has acted on guidance provided by the National Planning Policy Framework, specifically paragraphs 186 and 187. The local planning authority has worked proactively with the applicant to bring forward a high quality development that will improve the economic, social and environmental conditions of the area.

These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.

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PLANNING COMMITTEE

Date: 6th March 2013

Application Number	12/1537/FUL	Agenda Item	
Date Received	5th December 2012	Officer	Mr Sav Patel
Target Date	6th March 2013		
Ward	Kings Hedges		
Site	Land to the rear of 231-247 Milton Road Cambridge CB4 1XQ		
Proposal	Erection of 13 no. dwellings (following demolition of no. 235 Milton Road)		
Applicant	Mr c/o Agent		

SUMMARY	<p>The development accords with the Development Plan for the following reasons:</p> <ul style="list-style-type: none"> <input type="checkbox"/> The proposed development would contribute towards meeting the current housing needs; <input type="checkbox"/> The proposed development has been designed and laid out to complement the setting without have a detrimental impact on the amenity of the surrounding residents. <input type="checkbox"/> The proposal would make effective and more efficient use of this constrained site in terms of building layout, design and in terms of housing need;
RECOMMENDATION	APPROVAL

1.0 SITE DESCRIPTION/AREA CONTEXT

- 1.1 The application site covers 0.49 hectares and is located to the rear of no.231 to 247 Milton Road. The site is an amalgamation of nine ancillary rear gardens, on the north-western side of Milton Road. The site also backs onto the rear boundaries of no.31 to 43 Woodhead Drive.

- 1.2 The application site currently consists of overgrown vegetation and groups of mature trees. There is a group of trees within the site, which are protected by a Tree Preservation Order (40/1988). However, the Order certificate, the does not define the location of trees but does describe the specimens.
- 1.3 The area is characterised by low density, mainly detached two storey properties set back from the highway on large plots. The plot depths are on average 80 metres front to back. Some of the rear gardens along Milton Road (for examples 233, 229 and 283 Milton Road) have already been subdivided in a piecemeal fashion to accommodate new, back land dwellings. This is most prevalent on the application site side of Milton Road.
- 1.4 On the opposite side of Milton Road (south-east) is a consistent pattern of two storey semi-detached houses set back from the road.
- 1.5 To the north-west of the site are the two storey dwellings in Woodhead Drive and three storey block of residential flats in Robert Jennings Close. To the north of the properties on Woodhead Drive is a recent residential development comprising two and three storey properties.
- 1.6 To the north-east of the site is the site boundary of no.249 Milton Road.
- 1.7 To the south-east of the site are the retained rear gardens of no.231 and to 247 Milton Road.
- 1.8 To the south-west of the site is the side boundary of no.229a Milton Road and rear boundaries no.231 and 233 Milton Road. No.229a is a single storey dwelling in a back land location. To the south-west of no.229a is 227a which is another back land development dwelling.

2.0 THE PROPOSAL

- 2.1 The proposal is for the demolition of no.235 Milton Road and the development of 13 dwellings served by a new shared surface access road off Milton Road, and the demolition of no.235 Milton Road. The development is a mix of six 3bed, five 4bed and two 5bed houses. The proposed dwellings

comprise semi-detached and detached units with attached or separate garage units. Ten of the proposed dwellings would include loft rooms (plots 1, 2, 4, 5, 6, 7, 10, 11, 12, and 13). Each dwelling would also include roof integrated solar panels.

- 2.2 The loft rooms would contain high-level rooflights with minimum cill heights of 1.75 metre.
- 2.3 Plots 2, 5, 6, 9, 10, 11, 12 and 13 have attached garages, which are proposed to contain a room in the roof space. The attached garages of plots 3 and 9 would contain dormer windows facing into the development site. Plots 1, 4, 7 and 8 would have single storey detached garages.
- 2.4 A pre-application meeting was held on 8 October 2012 with the applicant to discuss the proposed development. The scheme was then amended following consultation responses from consultees who attended the meeting such as Urban Design, Landscaping/Trees, Drainage and County Highways. Further pre-application discussions were with held to address detailed design and layout issues.
- 2.5 The application is accompanied by the following supporting information:
 1. Planning Statement;
 2. Design and Access Statement including shadow assessment;
 3. Tree Survey;
 4. Arboricultural Assessment
 5. Preliminary Method Statement;
 6. Tree Daylight Study;
 7. Executive Summary – Tree Survey and Arboricultural Impact Assessment;
 8. Drainage Design Statement;
 9. Foul and Utilises Assessment;
 10. Phase 1 Design Study and Ground Investigation Report;
 11. Transport Statement;
 12. Sustainability Statement and checklist;
 13. Site Waste Management Plan;
- 2.6 Amended plans have been received which show the following revisions:

- ☐ Footway either side of the access has been increased to connect to the shared surface;
- ☐ Shadow study submitted to demonstrate the north-west facing gardens of plots 4-8 will receive sufficient light;
- ☐ Garage sizes have been increased to accommodate cycle parking provision;
- ☐ Bin storage areas will be bound by timber trellis;
- ☐ The garage for plot 3 had been pulled away from the rear boundary with no.39 by 1.5 metres thus increasing the distance to 2.8 metres off the boundary;
- ☐ The rooflights in plots 1, 2, 4, 5, 6, 7, 10, 11, 12 have been set at 1.75 metres cill height to restrict overlooking.

2.7 The application is brought before Planning Committee because it is a major planning application, which has received objections from local residents during the neighbourhood consultation process.

3.0 SITE HISTORY

Reference	Description	Outcome
C/88/0661	RESIDENTIAL DEVELOPMENT (OUTLINE APPLICATION).	10.08.1988

4.0 PUBLICITY

4.1	Advertisement:	Yes
	Adjoining Owners:	Yes
	Site Notice Displayed:	Yes
	Public Exhibition (meeting of 1/11/12):	Yes

5.0 POLICY

5.1 Central Government Advice

See Appendix 1 for full details of Central Government Guidance, East of England Plan 2008 policies, Cambridgeshire and Peterborough Structure Plan 2003 policies, Cambridge Local Plan 2006 policies, Supplementary Planning Documents and Material Considerations.

5.2 Relevant Development Plan policies

PLAN	POLICY NUMBER
Cambridgeshire and Peterborough Structure Plan 2003	P6/1 P9/8 P9/9
Cambridge Local Plan 2006	3/1 3/3 3/4 3/6 3/7 3/10 3/11 3/12 4/3 4/4 4/8 4/13 4/14 4/15 5/1 5/5 8/1 8/2 8/4 8/6 8/9 8/10 8/11 8/16

5.3 Relevant Central Government Guidance, Supplementary Planning Documents and Material Considerations

Central Government Guidance	National Planning Policy Framework March 2012 Circular 11/95 Community Infrastructure Levy Regulations 2010
Supplementary Planning Documents	Sustainable Design and Construction Waste Management Design Guide Planning Obligation Strategy Public Art
Material Considerations	<u>Central Government:</u> Letter from Secretary of State for Communities and Local Government (27 May 2010) Written Ministerial Statement: Planning for Growth (23 March 2011)

	<p><u>Citywide:</u></p> <p>Arboricultural Strategy</p> <p>Biodiversity Checklist</p> <p>Cambridge Landscape and Character Assessment</p> <p>Cambridge City Nature Conservation Strategy</p> <p>Cambridge and Milton Surface Water Management Plan</p> <p>Balanced and Mixed Communities – A Good Practice Guide</p> <p>Green Infrastructure Strategy for the Cambridgeshire Sub-Region</p> <p>Cambridgeshire Quality Charter for Growth</p> <p>Cambridge Walking and Cycling Strategy</p> <p>Protection and Funding of Routes for the Future Expansion of the City Cycle Network</p> <p>Cycle Parking Guide for New Residential Developments</p> <p>Air Quality in Cambridge – Developers Guide</p> <p>Roof Extensions Design Guide</p>
	<p><u>Area Guidelines:</u></p> <p>Northern Corridor Area Transport Plan</p>

6.0 CONSULTATIONS

Cambridgeshire County Council (Highways)

- 6.1 The Highways Officer originally raised the following concerns with the scheme:

- ☐ A footway should be provided either side of the shared access;
 - ☐ Shared surface up to the bell mouth is inappropriate as pedestrian would be at serious risk of vehicles turning into the site;
 - ☐ The proposed 'Give Way' road marking would appear confusing and would not countenance such marking off Milton Road;
 - ☐ 2 m by 2 m visibility splays at the access is inadequate if intention is to allow pedestrians and cyclists prior across the junction;
 - ☐ Provide dimensions of the car parking spaces (2.5 metre x 5 metres) and garages (internal 6 metre x 3 metres);
 - ☐ Internal road layout is unsuitable for adoption as a public highway and will only be up to adoptable standard following considerable alteration. However, it is understood that the applicant is proposing a private road;
 - ☐ Refusal recommended unless two footways of suitable width are provided to bring pedestrians to a suitable, safer location.
- 6.2 In terms of the vehicular access arrangements for the site, the applicant has satisfied the County Highways Officer's main concern by proposing footpaths either side of the shared access.
- 6.3 The access road also provides provision for service vehicles to drive and turn within the site. A vehicle swept path has been provided in the applicant Transport Statement.
- 6.4 Each proposed dwelling would contain two car parking spaces, which is in accordance with the Car Parking Standards.
- 6.5 The County Highways Authority are satisfied with the highway arrangement subject to them not being responsible for maintaining the shared surface access, which should be secured in a S106 agreement.

Head of Environmental Services

- 6.6 No adverse comments, subject to the following conditions:
- ☐ A scheme for dust mitigation measures;
 - ☐ A scheme for mitigation of construction/demolition noise, vibration and piling;

- ☐ Restricted construction hours;
- ☐ Restricted collection and delivery hours;
- ☐ A scheme for refuse collection;
- ☐ Full land contamination assessment.

Environment Agency

6.7 No adverse comments subject to the following condition:

- ☐ Land contamination risk assessment

Sustainable City (Drainage)

6.8 Surface water and foul drainage solution would be acceptable for this site. Condition relating to this is recommended.

Cambridgeshire County Council (Archaeology)

6.9 No adverse comments subject to a condition requiring a programme of archaeological investigation works.

Cambridge City Council Access Officer

6.10 I have reservations about the long, narrow shared space driveway, it could be dangerous and confusing for visually impaired pedestrians.

The doors and French doors all appear to be one step, not flat thresholds.

Nature Conservation Officer

6.11 Phase 1 habitat and species survey suggests no protected species or habitats have been identified, however the proposal would result in loss of significant area of mature gardens. Enhancements measures are proposed including planting of native trees and erection of bird and bat boxes.

A condition is recommended requesting all enhancement measures are provided and positioned by suitably qualified ecologist. In addition, access for hedgehogs and amphibians is maintained below any new garden fencing.

Landscape

6.12 The landscape officer has the following concerns with the proposed scheme:

- ☐ Impact of proposed development on neighbouring properties; create a sense of enclosure and reduce privacy on amenity areas;
- ☐ Removal of trees and woodland shrub;
- ☐ Insufficient space within the proposed site layout to accommodate larger trees;
- ☐ Future pressure to remove trees in plot 8 and 9 due to shading.

However, if officers are minded to approve then they have recommended the following conditions:

- ☐ Full soft landscaping details;
- ☐ Full hard landscaping details;
- ☐ Maintenance plans for entire site

Trees Officer

6.13 No comments received. Any comments will be reported on the amendment sheet or verbally to Members at Committee.

Urban Design and Conservation

6.15 A series of concerns were raised with the proposed development relating to the arrangement of the dwellings along the northern boundary receiving limited daylight and requested a shadow impact assessment to be carried out. Other concerns related to the following issues:

- ☐ Extend the proposed footpath to connect with the shared surface;
- ☐ Hand plots 1 and 2 to provide better elevational composition;
- ☐ Amend depths of the garages to comply with the Cycle Parking Guide for New Residential Developments (Feb 2010);
- ☐ Clarify the proposed materials for plot 8;
- ☐ Provide properly designed refuse storage structures within the rear gardens of all units.

6.16 Following the submission of amended plans and a shadow study, Urban Design Officers are now satisfied with the proposed scheme.

6.17 The above responses are a summary of the comments that have been received. Full details of the consultation responses can be inspected on the application file.

7.0 REPRESENTATIONS

7.1 The owners/occupiers of the following addresses have made representations:

- ☐ 10 Downhams Lane;
- ☐ 229a Milton Road;
- ☐ 237a Milton Road;
- ☐ 283a Milton Road;
- ☐ 286/288 and 290 Milton Road;
- ☐ 314 Milton Road;
- ☐ 320 Milton Road;
- ☐ 9 Robert Jennings Close;
- ☐ 39 Woodhead Drive;
- ☐ 31 Woodhead Drive;
- ☐ 33 Woodhead Drive;
- ☐ 37 Woodhead Drive;
- ☐ 41 Woodhead Drive;
- ☐ 43 Woodhead Drive;

7.2 Councillor Price has also made representations.

7.3 The representations can be summarised as follows:

- ☐ Development out of proportion and keeping with area;
- ☐ Scale and density of development/overdevelopment;
- ☐ Backland development;
- ☐ Impact on privacy from overlooking;
- ☐ Loss of outlook and shading;
- ☐ Overbearing sense of enclosure;
- ☐ Traffic noise/disturbance and highway safety;
- ☐ Loss of existing wildlife/biodiversity and trees;

7.4 The above representations are a summary of the comments that have been received. Full details of the representations can be inspected on the application file.

8.0 ASSESSMENT

8.1 From the consultation responses and representations received and from my inspection of the site and the surroundings, I consider that the main issues are:

1. Principle of development
2. Context of site, design and external spaces
3. Public Art
4. Renewable energy and sustainability
5. Disabled access
6. Residential amenity
7. Refuse arrangements
8. Highway safety
9. Car and cycle parking
10. Third party representations
11. Planning Obligation Strategy

Principle of Development

- 8.2 The application site is located in a sustainable location due to its proximity to local shops and public transport and cycle links into the city centre. Windfall housing sites such as this are permitted by the Local Plan subject to the existing land uses and compatibility with adjoining uses.
- 8.3 The proposed development would complement the existing form and character of development in the area. There are also other examples of back land development along Milton Road, albeit on a smaller scale to that proposed here. Nevertheless, the principle of residential development on ancillary garden land on this side of Milton road has been established.
- 8.4 In my opinion, the principle of the development is acceptable and in accordance with policies 3/4 and 5/1.

Context of site, design and layout and external spaces

Context of site:

- 8.5 The application site comprises an amalgamation of garden land serving nine plots (no.231/233/237/239/241/243/245/247 and 249), which has become overgrown and contains several

mature trees some of which are protected by a Tree Preservation Order.

- 8.6 The built form of the area is characterised by mainly two storey detached properties set back from the highway and on generous plots. Some of the existing properties adjacent to the application site have subdivided their rear gardens to accommodate additional dwellings in a back-land setting. Policy 3/10 (sub-division of existing plots) is specifically related to this form and type of development. It states that residential development within the garden area or curtilage of existing properties will not be permitted if it:
- a) has a significant adverse impact on the amenities of the neighbour properties through loss of privacy, loss of light, an overbearing sense of enclosure or noise nuisance;
 - b) provides inadequate amenity space for proposed and existing properties;
 - c) detracts from the prevailing character and appearance of the area;
 - d) adversely affects the setting of Listed Buildings etc..
 - e) adversely affects trees, wildlife features or architectural features or local importance located within or close to the site;
 - f) prejudice the comprehensive development of the wider area of which the site forms part.

- 8.7 Taking each point separately, I set out below my assessment of the proposal against this relevant policy:

Policy 3/10 (a) – impact on residential amenity

- 8.8 Careful consideration has been given to the sensitive context of the site being located to the rear of two linear forms of residential development.
- 8.9 Whilst it is expected that any form of development on this site would have a degree of impact on the surrounding properties from the impact of overlooking, perception of new buildings (including replaced outlook) and noise disturbance from

additional traffic movements, I believe the applicant has identified the main constraints and mitigated any adverse harm through carefully designing and laying out the properties and site.

- 8.10 In terms of the impact on residential amenity on surrounding properties, the internal arrangement of the proposed dwellings has been carefully laid out, particularly at first floor level.
- 8.11 Plots 1 & 2, which back onto the side boundary of 229a Milton Road (single storey property) have one clear window in the rear elevation at first floor serving a bedroom that would face the rear boundary. The other window at first floor would serve a bathroom and would be obscurely glazed. The plots would also contain one bedrooms in the roofspace. Each bedroom would be served by 3 pane rooflights, set at 1.75m to the cill in order to prevent directly views of the side boundary and amenity area of no.229a. Furthermore, the properties in plots 1 and 2 would be set away from the rear boundary by 10.3 metres and face onto the side elevation of no.229a which has a small window in the side gable end. The side boundary of the 229a appears to be defined by a combination of dense hedgerow trees, which enclose the rear garden area. The proposing would provide a 1.8 metre high close-boarded fence adjacent to it.
- 8.12 I am of the view that the proposed dwellings would not have a significantly adverse impact on the residential amenity of the occupier of no.229a.
- 8.13 Plot 3 would face the private amenity area of no.229a. However, like with plots 1 and 2, the internal arrangement of plot 3 has been carefully laid out so as to minimise impact on residential amenity. The proposed dwelling on plot 3 is an 'L' shape building with a 1 ½ storey attached garage. In the main two storey element of the property, there are two bedroom windows at first floor that would face the side boundary of no.229. The bedroom windows would be located approximately 15.2 metres from the side boundary. The applicant is also proposing to retain the existing group of trees (T006 and G001) to further mitigate the views over the private amenity area. There is also a bathroom window, which would be obscurely glazed.

- 8.14 I am of the view that the degree of impact from the first floor bedroom windows in plot 3 would not have a significantly adverse impact on the residential amenity of the occupiers of no.229a.
- 8.15 As for no.231 and 233 Milton Road, there are no clear windows in the gable end of plot 1 at first or roofspace level that would cause overlooking of the rear gardens. No.231 and no.233 benefit from approximately 33 metres rear gardens. Furthermore, rear boundaries of these properties are defined by dense tree and vegetation would screen views of plot 1.
- 8.16 As for the impact on traffic movement along the access road, the applicant is proposing to install a 1.8 metre high acoustic barrier along the side boundary of no.233. The applicant is also proposing to incorporate landscaping either side of the access road, which will help to attenuate noise overspill onto no.233. Details of the full soft and hard landscaping will be conditioned for approval. Each proposed dwelling would benefit from 2 car parking spaces. This would mean a total of 26 cars would be able to enter and leave the site. The County Highways Officer did not raise the level of vehicle movement associated with the proposed development as an issue.
- 8.17 Turning to the properties in Woodhead Drive, the proposed development extends across the rear boundaries of no.31 to no.41. A 1.5 metre section of the site would extend onto the rear boundary no.43. However, I do not consider the proposed development would directly impact no.43 due to its location from the site approximately 13 metres, the existing tree coverage and distance from plot 3 (approximately 23 metres – rear corner to rear corner).
- 8.18 The side elevation of plot 3 would back onto the rear boundaries of no.41 and no.39 Woodhead Drive. There would be one bedroom window that would face the rear boundary with no.41, which would be set back approximately 11.1 metres. No. 41 has a garden depth of 12.3 metres. Therefore, the level of window to window separate would be 23.4 metres. Furthermore, the bedroom window would be the only first floor window in plot 3 that would face the rear boundary of no.41 and 39. Therefore, I do not consider plot 3 would have a significantly adverse impact on the residential amenity in terms of overlooking on no.39 or no.41 Woodhead Drive.

- 8.19 However, plot 3 would have a closer relationship with no.39 in terms of separation distances than any other plot with the proposed development. The proposed dwelling was originally set away from the rear boundary with no.39 by 1.5 metres. The attached garage block was set off the boundary a further 1.5 to give a greater separation without materially adjusting the design of the proposed dwelling. The separation from the boundary, following the amendments would be 3 metres. This equates to an 18 metre distance between no.39 and the proposed dwelling.
- 8.20 The main impact from the proposed dwelling in plot 3 would arise from the two storey hipped gable element. The gable would be 7.6 metres to the ridge and 5.1 metres to the eaves line. According to the applicant shadow study, the only significant shadowing of the rear garden would occur during December at 09:00. This is the time at which all the properties that back onto the application from Woodhead Drive would be affected. The shadowing largely disappears from the rear gardens at 12:00. It is important to note here that the existing trees coverage within the site already causes a certain level of shadowing which is further exacerbated during summer months when the trees are in leaf. Therefore, whilst it is accepted there will be a visual presence impact on the occupier of no.39, I do not consider this would have an unacceptable detrimental impact on the amenity of no.39.
- 8.21 Between the rear boundaries of no.37 and 39 would be the single storey hipped roof detached garage for plot 4 located approximately 800mm from the rear boundary. The hipped roof of the garage would slope away from the boundary to a height of approx 4.1 metres to the ridgeline and approx 2.4 metres to the eaves line. I do not consider the garage would have any detrimental impact on the amenity of the occupiers of no.37 and 39.
- 8.22 The proposed dwellings in plot 4 and 5 would comprise two detached properties with rooms in the roofspace and back onto the rear boundary of no.37. No.37 is set approx 3.3 metres further forward (nearer Woodhead Drive) than no.39. Therefore the rear garden depth is approx 19 metres. The properties in plots 4 and 5 would have rear garden depths of approx 9 metres. This means a separate distance of 28 metres between the proposed dwellings in plot 4 and 5 and no.37.

- 8.23 Again as with plots 1 and 2, the applicant has carefully arranged the internal layout of the properties and located the bathrooms to the rear thus leaving 1 clear bedroom window in the rear elevation at first floor level. Therefore, in conjunction with separation distance between no.39 and, plots 3 and 4 and the applicant's proposal to incorporate boundary treatment along the rear gardens, I do not consider that a single bedroom window at first floor level in this urban setting would have a significantly unacceptable impact on the amenity of no.37.
- 8.24 The rooflights throughout the proposed development would have a cill height of 1.75 metres. Therefore, the unintentional ability to stand and look directly out of the windows over the private garden areas of the properties in Woodhead Drive would not be possible.
- 8.25 The applicant has responded to the concerns raised by the local residents and believes although the rooflights could be transferred to the front elevation but it was felt that this would affect the external appearance of the proposed dwellings. The applicant believes the raised cill heights are sufficient enough to overcome the overlooking issue.
- 8.26 Plots 6 and 7 comprise semi-detached properties, which extend across the rear boundaries of no.33 and no.35 Woodhead Drive. The rear gardens of both proposed plots are located approx 9 metres from the rear boundary fence of both existing properties. However, no.35 is set approx 6 metres further forward than no.33. This gives no.35 a rear garden depth of 20.1 metres. No.33 has a 12.3 metres deep rear garden. The separate distance between no.35 and dwelling in plot 6 and 7 would be approx 29 metre. The separate distance between no.33 would be approx 21.6 metres.
- 8.27 Again, as with plots 1, 2, 3, 4 and 5 above, the first floor windows in the rear elevations facing the rear boundary of no.33 and no.35 serve a bathroom and bedroom. Therefore, the level of overlooking would not be significant particularly due to the level of separation between the existing and proposed dwellings. The applicant is also proposing to landscape the rear boundary, which is help to further mitigate views across the rear boundary.

- 8.28 The proposed single storey garage block for plot 7 and 8 would be located 1.8 metres from the rear boundary of no.33. The garage block would approx 8.65 metres wide with a hipped roof form. The ridge height would be approx 5.2 metres and approx 2.45 metres to the eaves. Whilst the garage building will be visible from the rear garden of no.33 I do not considered it would significantly harm the residential amenity of the occupiers in terms of being overbearing or create a sense of enclosure due to the distance of separate being approx 14.2 metres.
- 8.29 Plot 8 comprises a detached 'T' shape dwelling located approx 13 metres from the rear boundary of no.33 and 31 Woodhead Drive. The proposed dwelling extends across most of rear boundary of no.31. No.31, as with no.35 and 37, is set further forward providing a rear garden depth of 20.1 metres. The separate distance between the dwellings would be approx 30.6 metres. The separate distance between no.33 would be approx 25.4 metres.
- 8.30 The rear elevation of plot 8 contains two separate bedroom windows and a landing window at first floor level. The nearest bedroom window (second bedroom) on the rear boundary would be approx 10.3 metres away from the rear boundary whereas the landing and third bedroom window would be located approx 13 metres away from the rear boundary. Again, whilst there is likely to be some form of inter-visibility of rear gardens, I believe the level of separation, in this context, between the existing and proposed dwellings would mitigate any detrimental harm. There is an existing Ash tree close to the rear boundary of no.31, which the applicant is proposing to retain. The Ash has a visually dominant broad crown, which will help to mitigate overlooking issues.
- 8.31 Plots 9 and 10 would comprise a two storey property and a two ½ storey (loft room) property, respectively. These proposed dwellings would be located in the northern corner of the site and well screened from no.31 Woodhead Drive by existing trees.
- 8.32 Concerns were raised with the amount of day light that plot 9 would receive due to the proposal to retain the existing boundary trees within the plot. The plot comprises two mature Beech trees and one mature Sycamore tree. There is also a cluster of other trees within a landscaped buffer zone in the

Robert Jennings Close. These will not be affected by the proposed development.

- 8.33 The applicant's shadow study demonstrates that the rear garden of plots 9 and 10 would be in shade in late December months and particularly at 15:00. During late March and late June the garden areas for these plots would receive light. Based upon this evidence, I am satisfied that the shadowing impact from the boundary trees would not be constant albeit would be significant during the winter months.
- 8.34 I am also satisfied that neither proposed dwelling in plot 9 and 10 would cause any unacceptably adverse overlooking of surrounding gardens due to the orientation of the properties with surrounding properties and existing garden depths. For example, the proposed dwelling in plot 9 would be located approx 15 metres from the side boundary with no.249 and plot 10 would be located approx 11.8 metres from the side boundary. The rear boundary of plot 10 would be located approx 49 metres along the side boundary of no.249. It would be unreasonable to argue that the end most part of a garden, which stretches over 80 metres, constitutes private amenity space.
- 8.35 Finally, plots 11, 12 and 13 comprise a semi-detached unit (11 & 12) and a detached unit (13). All three properties would contain rooms in the roof of the attached garages and roof of the main dwelling. None of the proposed dwellings are considered to cause any adverse residential amenity issues on existing occupiers due to the depths of the existing gardens, particularly of those properties that form the rear boundary with plots 11 and 12 and due to the orientation of proposed dwelling in plot 13, which faces the side elevation of the dwelling in no.12. The dwellings in plot 11 and 12 would be located approx 6.3 metres from the rear boundary, which would be shared partially with with no.237 and entirely with no.239 Milton Road. The separate distances between the rearmost aspect of no.239 and proposed dwellings would be 32 metres. The dwelling in plot 12 would be located approx 40 metres from the rear elevation of no.237. The side elevation of the dwelling in plot 13 would be located approx 32 metres from the rear elevation of no.237a and approx 33.4 from no.237 Milton Road.

8.36 To conclude on this point, whilst the proposed development will have some degree of impact on the surrounding properties in terms of perception and the presence of new buildings, I do not consider the degree of impact would cause such significantly unacceptable harm on the residential amenity to warrant the refusal of this application on this basis alone.

8.37 I have assessed each proposed dwelling against how it would relate to the existing properties that surrounding the site and do not consider the impact on each adjacent house would be materially detrimental enough to justify refusal of the application. Therefore, I am satisfied that this aspect of the Policy 3/10 has been complied with.

Policy 3/10 (b) – amenity space, access and car parking

8.38 Each proposed dwelling has been provided with amenity private amenity space, albeit some with more space than others. The level of amenity space in plots 11 and 12 are the smallest but at approx 6.3 metres deep this is considered to be acceptable. I am satisfied that the proposed development would provide adequate amenity space for each dwelling.

8.39 The nine rear gardens that would be used to make up the application site would still retain sizeable amenity areas ranging from 32.4 metres (no.231 Milton Road) to 49 metres (245 Milton Road).

8.40 No form of open space has been proposed on the site. This is mainly due to the scale of the development and site constraints. The site is located close to areas of open space provided on the large residential development on Woodhead Drive, which could be utilised.

8.41 Aside from this, the applicant has accepted making a commuted payment to the various forms of open space provision and community facilities, as required by the Council's Planning Obligations Strategy.

Policy 3/10 (c) – prevailing character and appearance of the area

8.42 The prevailing character of the area comprises a mix of post-war low density housing some of which have been subdivided

to provide plots with back land housing, a new high density housing estate and three storey flat block developments. The built form of the immediate area is characterised by mainly two storey dwellings set back from the adjoining highway. However, there are also some examples of single storey bungalow type dwellings. Also, the recent housing development to the north of Woodhead Drive contains two and three storey housings. The overall character of the area is mixed in age and style.

8.43 The existing properties fronting Milton Road are of varied design and appearance, and the proposal picks up on the 1920/1930's suburban vernacular of Milton Road informed by the local palette of materials and details; tiled hipped roofs, facing brickwork, hung tiles, square bay windows, etc, but used in a more contemporary composition to create a distinctive environment.

8.44 In terms of the external materials, the applicant has opted, rather than contrasting materials between plots, subtle variations in materials. It is felt that this approach would lead to a more visually coherent scheme. The proposal is seeking to achieve a hierarchy of materials to match the hierarchy of buildings will be promoted to ensure that each dwelling has its own identify, whilst retaining an overall coherence and simplicity to the pallet of materials.

8.45 The proposed dwellings have been designed to compliment the style and character of the area by incorporating a traditional approach including pitched and hipped roofs and box bay windows. The Urban Design Officer supports the elevational treatment of the dwellings and considers the proposed scale and massing to accord with the prevailing height of surrounding residential development.

8.45 I am satisfied that the proposed development would complement the built form of the area and therefore comply with this aspect of Policy 3/10.

Policy 3/10 (d) – Listed Building

8.46 The property to be demolished is not a Listed Building or a BLI and there are no such buildings within close proximity to the site.

Policy 3/10 (e) – Loss of wildlife/trees

- 8.47 The applicant has assessed the conditions of the existing trees and biodiversity of the site and has submitted a Tree Survey and Arboricultural Implication Assessment and Extended Phase 1 Habitat survey.
- 8.48 A group of ten trees within the site are protected under a Tree Preservation Order (40/1988). The group includes 1 Ash, 1 Poplar, 1 Sycamore, 2 Robinia, 1 Pine, 1 Spruce, 2 Walnut and 1 Silver Birch. However, the Council's records do not show where these trees are located on a map.
- 8.49 Sixty four trees, six groups, five hedges and four areas were identified and assessed in the applicants tree survey. These were found to be of mixed condition and age. However, none of the existing trees were identified as being Category A (high quality) specimens or landscape features. Only fifteen trees were identified as being Category B (modest quality) trees. According to the tree survey, 81% of the trees and landscape features identified within the site are in poor form, low quality and unsuitable for retention. These mainly relate to smaller specimens.
- 8.50 Thirty six trees, four groups of trees and 5 hedges are required to be felled to accommodate the scheme. The remaining trees and landscape features will be protected from construction development through root protection zones and specialised foundation techniques during site preparation works. The survey highlighted that none of the proposed foundations for the buildings would fall within any of the Root Protection Areas.
- 8.51 This will inevitably and noticeable affect the landscape and appearance of the area. However, the tree consultant advises that the proposed scheme represents a realistic opportunity for landscape improvements, which may not otherwise have occurred. Therefore, I am satisfied that the loss of trees and landscape features will be mitigated by the opportunity to enhance the landscape attributes of the site which would be achieved via detailed soft and hard landscaping conditions.
- 8.52 The delivery of much needed new housing stock with the City, is considered to outweigh the detrimental impact on the landscape amenity of the area. The phase 1 study has

demonstrated that there are no protected species within the rear gardens of the nine plots. Therefore, the with careful removal of the identified trees and vegetations with the replacement trees and landscaped boundaries planting including provision for bats and birds, I believe the proposed development would sympathetically assimilate into the site whilst also providing wildlife benefits.

Policy 3/10 (f) – prejudice the comprehensive development of the wider area

8.53 The proposed site area is land locked from all sides and therefore there are no further rear gardens available for development, which could be linked to from the application site. On this basis, the proposal would not prejudice the development of adjoining rear gardens for other properties along Milton Road. I am satisfied that the proposed development would represent comprehensive development of available land in this specific location.

8.54 I am of the view that whilst the proposed development will materially affect the appearance of the site and have a degree of impact on the residential amenity of the neighbouring occupiers, the adverse impacts are not significant enough to outweigh the merits of the scheme such as making effective and efficient use of ancillary garden land to provide much need housing stock of a design and layout that would complement the character of the area.

8.55 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 3/4, 3/10, 3/11 and 3/12.

Public Art

8.56 The applicant has not put forward a public art scheme for the site but has agreed to either making a commuted payment for off site provision or an on-site contribution. This flexible public art solution has been agreed by the Council's Public Art Officer and will therefore be incorporated into the S106 agreement.

8.57 In my opinion the proposal is compliant with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and 9/8 and Cambridge Local Plan (2006) policies 3/7 and 10/1 and the Public Art SPD 2010

Renewable energy and sustainability

- 8.58 The applicant has considered a range of renewable energy provisions for the site such as wind generation, ground source heat pumps and PV cells but believes Solar Water Heating would be the most effective for this scale of development and constraints of the site.
- 8.59 The applicant has shown the solar panels on the pitched roof areas of dwellings and/or garages with a southwest and northwest aspect. The panels would provide 50-60% of the annual hot water requirements for the proposed dwellings.
- 8.60 The applicant Sustainability Report advises, using based upon baseline figures, the proposed solar panels would help to reduce carbon emissions from the development by 10%. Whilst this does not demonstrate 10% of energy from renewable sources, the applicant is proposing to undertake a full SAP calculation in order to establish the 10% renewable requirement taking passive solar energy into account, should the application be approved.
- 8.61 The comments from the Council's Sustainability Officer will either be reported on the amendment sheet or reported verbally to Members at the Committee meeting.

Disabled access

- 8.62 The proposed access would have pedestrian access lanes either side of the private road leading to the shaded surface highway that would enable safe access from the shared surface to Milton Road. The applicant is proposing design/define the road edge by means of a lipped kerb and tonal contrast to aid the visually impaired. All front doors of the proposed dwellings will be ramped/level access to aid wheel chair users.
- 8.63 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 3/7 and 3/12.

Residential Amenity

- 8.64 I have addressed the residential amenity issue in paragraphs 8.8-8.37 above.

- 8.65 In my opinion the proposal provides a high-quality living environment and an appropriate standard of residential amenity for future occupiers, and I consider that in this respect it is compliant with Cambridge Local Plan (2006) policies 3/7 and 3/12 *(or 3/14)*.

Refuse Arrangements

- 8.66 Each proposed dwelling would be provided with three 240Ltr wheelie bins allocated for green waste, dry recyclable and residual waste. This is in accordance with the RECAP Waste Management Design Guide.
- 8.67 The applicant has submitted a Site Waste Management Plan to comply with the requirements of the Site Waste Management Plan Regulation 2008.
- 8.68 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policy 3/12.

Highway Safety

- 8.69 The proposed access arrangements have been designed to ensure there are no adverse highway safety issues. The junction onto Milton Road will provide a 6 metre wide and 18 metres deep private road before turning into a shared surface highway. There will be footpaths either side of the private road leading to the shared surface to avoid conflict between pedestrians and cars, as requested by County Highways. The shared surface is proposed to 5.5 metres wide, which narrows to 4.8 metres before widening to 5.5 metres further into the site.
- 8.70 The County Highways Officer is satisfied with the highway and access arrangements for the proposed development.
- 8.71 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policy 8/2.

Car and Cycle Parking

- 8.72 The proposed development would provide two car parking spaces for each dwelling, which is in accordance with the Car

Parking Standards. Although no dedicated visitor parking has been provided, there is the potential for vehicles to be parked in tandem on some of the plots. Nonetheless, the Highways Officer has not raised any adverse issues with the car parking arrangements for the proposed development.

- 8.73 The proposed development would also provide each dwelling with at least two secured cycle parking spaces within the garages. This would reflect the requirements of the Cycle Parking Guide for New Developments.
- 8.74 In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 8/6 and 8/10.

Third Party Representations

- 8.75 The material planning issues raised in the representation received have been addressed specifically in the Context of site, design and layout and external spaces, Residential Amenity, Refuse Arrangements, Highway Safety and Car and Cycle Parking sections of the report.

Planning Obligation Strategy

Planning Obligations

- 8.76 The Community Infrastructure Levy Regulations 2010 have introduced the requirement for all local authorities to make an assessment of any planning obligation in relation to three tests. If the planning obligation does not pass the tests then it is unlawful. The tests are that the planning obligation must be:

- (a) necessary to make the development acceptable in planning terms;
- (b) directly related to the development; and
- (c) fairly and reasonably related in scale and kind to the development.

In bringing forward my recommendations in relation to the Planning Obligation for this development I have considered these requirements. The Planning Obligation Strategy (2010) provides a framework for expenditure of financial contributions collected through planning obligations. The Affordable Housing

Supplementary Planning Document 2008 provides guidance in terms of the provision of affordable housing and the Public Art Supplementary Planning Document 2010 addresses requirements in relation to public art (amend/delete as applicable). The applicants have indicated their willingness to enter into a S106 planning obligation in accordance with the requirements of the Strategy and relevant Supplementary Planning Documents. The proposed development triggers the requirement for the following community infrastructure:

Open Space

- 8.77 The Planning Obligation Strategy requires that all new residential developments contribute to the provision or improvement of public open space, either through provision on site as part of the development or through a financial contribution for use across the city. The proposed development requires a contribution to be made towards open space, comprising outdoor sports facilities, indoor sports facilities, informal open space and provision for children and teenagers. The total contribution sought has been calculated as follows.
- 8.78 The application proposes the erection of five four-bedroom houses, six three-bedroom houses, two five-bedroom houses flats. One three-bedroom residential unit would be removed, so the net total of additional residential units is twelve. A house or flat is assumed to accommodate one person for each bedroom, but one-bedroom flats are assumed to accommodate 1.5 people. Contributions towards provision for children and teenagers are not required from one-bedroom units. The totals required for the new buildings are calculated as follows:

Outdoor sports facilities					
Type of unit	Persons per unit	£ per person	£per unit	Number of such units	Total £
studio	1	238	238		
1 bed	1.5	238	357		
2-bed	2	238	476		
3-bed	3	238	714	5	3570
4-bed	4	238	952	7	6664
Total					10234

Indoor sports facilities					
Type of unit	Persons per unit	£ per person	£per unit	Number of such units	Total £
studio	1	269	269		
1 bed	1.5	269	403.50		
2-bed	2	269	538		
3-bed	3	269	807	5	4035
4-bed	4	269	1076	7	7532
Total					11567

Informal open space					
Type of unit	Persons per unit	£ per person	£per unit	Number of such units	Total £
studio	1	242	242		
1 bed	1.5	242	363		
2-bed	2	242	484		
3-bed	3	242	726	5	3630
4-bed	4	242	968	7	6776
Total					10406

Provision for children and teenagers					
Type of unit	Persons per unit	£ per person	£per unit	Number of such units	Total £
studio	1	0	0		0
1 bed	1.5	0	0		0
2-bed	2	316	632		
3-bed	3	316	948	5	4740
4-bed	4	316	1264	7	8848
Total					13588

8.79 Subject to the completion of a S106 planning obligation to secure the requirements of the Planning Obligation Strategy (2010) and the Cambridge City Council Open Space Standards Guidance for Interpretation and Implementation (2010), I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and P9/8, Cambridge Local Plan (2006) policies 3/8 and 10/1 and the

Planning Obligation Strategy 2010 and the Cambridge City Council Open Space Standards Guidance for Interpretation and Implementation (2010)

Community Development

- 8.80 The Planning Obligation Strategy (2010) requires that all new residential developments contribute to community development facilities, programmes and projects. This contribution is £1256 for each unit of one or two bedrooms and £1882 for each larger unit. The total contribution sought has been calculated as follows:

Community facilities			
Type of unit	£per unit	Number of such units	Total £
1 bed	1256		
2-bed	1256		
3-bed	1882	5	9410
4-bed	1882	7	13174
Total			22584

- 8.81 Subject to the completion of a S106 planning obligation to secure the requirements of the Planning Obligation Strategy (2010), I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and P9/8, Cambridge Local Plan (2006) policies 5/14 and 10/1 and the Planning Obligation Strategy 2010.

Waste

- 8.82 The Planning Obligation Strategy (2010) requires that all new residential developments contribute to the provision of household waste and recycling receptacles on a per dwelling basis. As the type of waste and recycling containers provided by the City Council for houses are different from those for flats, this contribution is £75 for each house and £150 for each flat. The total contribution sought has been calculated as follows:

Waste and recycling containers			
Type of unit	£per unit	Number of such units	Total £
House	75	12	900

Flat	150		
Total			900

- 8.83 Subject to the completion of a S106 planning obligation to secure the requirements of the Planning Obligation Strategy (2010), I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and P9/8, Cambridge Local Plan (2006) policies 3/7, 3/12 and 10/1 and the Planning Obligation Strategy 2010.

Education

- 8.84 Upon adoption of the Planning Obligation Strategy (2010) the Council resolved that the Education section in the 2004 Planning Obligations Strategy continues to apply until it is replaced by a revised section that will form part of the Planning Obligations Strategy 2010. It forms an annex to the Planning Obligations Strategy (2010) and is a formal part of that document. Commuted payments are required towards education facilities where four or more additional residential units are created and where it has been established that there is insufficient capacity to meet demands for educational facilities.
- 8.85 In this case, twelve additional residential units are created and the County Council have confirmed that there is insufficient capacity to meet demand for pre-school education/primary education/lifelong learning. Contributions are therefore required on the following basis.

Pre-school education					
Type of unit	Persons per unit		£per unit	Number of such units	Total £
1 bed	1.5		0		
2+-beds	2		810	12	9720
Total					9720

Primary education					
Type of unit	Persons per unit		£per unit	Number of such	Total £

				units	
1 bed	1.5		0		
2+- beds	2				42000
Total					42000

Secondary education					
Type of unit	Persons per unit		£per unit	Number of such units	Total £
1 bed	1.5		0		
2+- beds	2		1520		
Total					

Life-long learning					
Type of unit	Persons per unit		£per unit	Number of such units	Total £
1 bed	1.5		160		
2+- beds	2		160	12	1920
Total					1920

8.86 Subject to the completion of a S106 planning obligation to secure the requirements of the Planning Obligation Strategy 2010, I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and P9/8, Cambridge Local Plan (2006) policies 5/14 and 10/1 and the Planning Obligation Strategy 2010.

Transport

8.87 Contributions towards catering for additional trips generated by proposed development are sought where 50 or more (all mode) trips on a daily basis are likely to be generated. The site lies within the North Corridor Area Transport Plan where the contribution sought per trip is £399

8.88 The applicants have submitted a transport assessment on which the following assessment of additional trips and contributions is based.

North Corridor Area Transport Plan				
Existing daily trips (all modes)	Predicted future daily trips (all modes)	Total net additional trips	Contribution per trip	Total £
		102	399	40698

8.89 Subject to the completion of a S106 planning obligation to secure this infrastructure provision, I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1, P9/8 and P9/9, Cambridge Local Plan (2006) policies 8/3 and 10/1 and the Planning Obligation Strategy 2010.

Public Art

8.90 See paragraph 8.56 above.

8.91 Subject to the completion of a S106 planning obligation to secure this infrastructure provision, I am satisfied that the proposal accords with Cambridgeshire and Peterborough Structure Plan (2003) policies P6/1 and 9/8, Cambridge Local Plan (2006) policies 3/7 and 10/1 and the Public Art SPD 2010.

Monitoring

8.92 The Planning Obligation Strategy (2010) requires that all new residential developments contribute to the costs of monitoring the implementation of planning obligations. The costs are calculated according to the heads of terms in the agreement. The contribution sought will be calculated as £150 per financial head of term, £300 per non-financial head of term. Contributions are therefore required on that basis.

Planning Obligations Conclusion

8.93 It is my view that the planning obligation is necessary, directly related to the development and fairly and reasonably in scale and kind to the development and therefore the Planning Obligation passes the tests set by the Community Infrastructure Levy Regulations 2010.

9.0 CONCLUSION

- 9.1 The proposed development is for thirteen, two storey and two and a half storey semi-detached and detached dwellings on land to the rear of no.231 to no.247. The proposal includes the demolition of the existing dwelling at no.235 Milton Road.
- 9.2 Prior to the submission of this planning application, the applicant held pre-application discussions with officers to assess the principle of development, the design and layout concept of the proposal, impact on the trees and wildlife and highways. Officers considered the principle of residential development on rear garden land to be acceptable, as the depths of the gardens and distances between each proposed dwelling to the existing dwellings was considered to be sufficient to accommodate this scale of development without adversely affecting the residential amenity of the adjoining occupiers.
- 9.3 Whilst it is noted that the proposed development will have a degree of impact the surrounding area in terms of appearance and impact on amenity, I do not consider the level of harm that would be created from the development to be such that it would justifiably warrant the application to be refused.
- 9.4 The proposed design and layout of the scheme has been carefully assembled and subject to conditions relating to landscaping to help improve the appearance of the site boundaries, I believe the scheme would make a positive contribution to the area.
- 9.5 The proposed development of new housing within the city, which makes effective and efficient use of ancillary garden land such as this would contribute towards meeting the currently housing needs for the City.
- 9.6 I consider the overall proposed scheme to be of high quality and believe it would have a positive impact on the character of the area in terms of its design and scale whilst contributing towards meeting the current housing needs. On this basis, the proposal is in accordance with policies 3/4, 3/10, 3/12 and 5/1 of the Cambridge Local Plan 2006.

10.0 RECOMMENDATION

APPROVE subject to the following conditions:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In accordance with the requirements of section 51 of the Planning and Compulsory Purchase Act 2004.

2. No development shall commence until a programme of measures to minimise the spread of airborne dust from the site during the demolition / construction period, including wheel washing, has been submitted to and approved in writing by the Local Planning Authority. The development shall be implemented in accordance with the approved scheme.

To satisfy the above the applicant should have regard to the control of dust and emissions from construction and demolition - Best Practice Guidance produced by the London Councils.

Reason: To protect the amenity of the adjoining properties during the construction period. (Cambridge Local Plan 2006 policy 4/13)

3. Prior to the commencement of the development hereby approved (including any pre-construction, demolition, enabling works or piling), the applicant shall submit a report in writing, regarding the demolition / construction noise and vibration impact associated with this development, for approval by the local authority. The report shall be in accordance with the provisions of BS 5228:2009 Code of Practice for noise and vibration control on construction and open sites and include full details of any piling and mitigation measures to be taken to protect local residents from noise and or vibration. Development shall be carried out in accordance with the approved details.

Due to the proximity of this site to existing residential premises and other noise sensitive premises, impact pile driving is not recommended.

Reason: To protect the amenity of the adjoining properties during the construction period. (Cambridge Local Plan 2006 policy 4/13)

4. Except with the prior written agreement of the local planning authority in writing no construction work or demolition shall be carried out or plant operated other than between the following hours: 0800 hours to 1800 hours Monday to Friday, 0800 hours to 1300 hours on Saturday and at no time on Sundays, Bank or Public Holidays.

Reason: To protect the amenity of the adjoining properties during the construction period. (Cambridge Local Plan 2006 policy 4/13)

5. Except with the prior agreement of the local planning authority in writing, there should be no collection or deliveries to the site during the demolition and construction stages outside the hours of 0700 hrs and 1900 hrs on Monday - Saturday and there should be no collections or deliveries on Sundays or Bank and public holidays.

Reason: To protect the amenity of the adjoining properties during the construction period. (Cambridge Local Plan 2006 policy 4/13)

6. No development shall commence until further details of the circulation route for refuse collection vehicles have been submitted to the local planning authority and approved in writing. The required details shall include a full construction specification for the route, and a plan defining the extent of the area to which that specification will be applied. No dwelling forming part of the development shall be occupied until the refuse vehicle circulation route has been laid out and constructed in accordance with the details thus approved, and thereafter the route shall be maintained in accordance with those details.

Reason: To protect the amenity of the adjoining properties during the construction period. (Cambridge Local Plan 2006 policy 4/13)

7. No development shall take place until a scheme for the provision and implementation of surface water drainage has been submitted to, and approved in writing by, the local planning authority. The scheme shall be constructed and completed according to the approved plans.

Reason: To avoid pollution. (Cambridge Local Plan (2006) policy 4/13)

8. No development shall take place until a scheme for the provision and implementation of foul drainage has been submitted to, and approved in writing by, the local planning authority. The scheme shall be constructed and completed according to the approved plans.

Reason: To avoid pollution. (Cambridge Local Plan (2006) policy 4/13)

9. No unbound material shall be used in the surface finish of the access within 20 metres of the highway boundary of the site.
Reason: To avoid displacement of loose material onto the highway in the interests of highway safety

10. Notwithstanding the provision of Class A of Schedule 2, Part 2 of the Town and Country Planning (General Permitted Development) Order 1995, (or any order revoking, amending or re-enacting that order) no gates shall be erected across the approved access unless details have first been submitted to and approved in writing by the Local Planning Authority.
Reason: In the interests of highway safety.

11. Prior to the commencement of the first use the vehicular access where it crosses the public highway shall be laid out and constructed in accordance with the Cambridgeshire County Council construction specification.

Reason: In the interests of highway safety and to ensure satisfactory access into the site.

12. The access shall be constructed with adequate drainage measures to prevent surface water run-off onto the adjacent public highway, in accordance with a scheme submitted to and approved in writing by the Local Planning Authority, in consultation with the Highway Authority.

Reason: To prevent surface water discharging to the highway.

13. The access shall be provided as shown on the approved drawings and retained free of obstruction.

Reason: In the interests of highway safety.

14. Before the development hereby permitted is commenced details of the following matters shall be submitted to and approved by the local planning authority in writing.
- i) contractors access arrangements for vehicles, plant and personnel,
 - ii) contractors site storage area/compound,
 - iii) the means of moving, storing and stacking all building materials, plant and equipment around and adjacent to the site,
 - iv) the arrangements for parking of contractors vehicles and contractors personnel vehicles.
 - v) movements and control of muck away lorries (all loading and unloading should be undertaken off the adopted public highway)

Thereafter the development shall be undertaken in accordance with the approved details.

Reason: To protect the amenity of the adjoining properties during the construction period. (Cambridge Local Plan 2006 policy 4/13)

15. No development shall take place until full details of both hard and soft landscape works have been submitted to and approved in writing by the local planning authority and these works shall be carried out as approved. These details shall include proposed finished levels or contours; means of enclosure; car parking layouts, other vehicle and pedestrian access and circulation areas; hard surfacing materials; minor artefacts and structures (eg furniture, play equipment, refuse or other storage units, signs, lighting); proposed and existing functional services above and below ground (eg drainage, power, communications cables, pipelines indicating lines, manholes, supports); retained historic landscape features and proposals for restoration, where relevant. Soft Landscape works shall include planting plans; written specifications (including cultivation and other operations associated with plant and grass establishment); schedules of plants, noting species, plant sizes and proposed numbers/densities where appropriate and an implementation programme.

Reason: In the interests of visual amenity and to ensure that suitable hard and soft landscape is provided as part of the development. (Cambridge Local Plan 2006 policies 3/4, 3/11 and 3/12)

16. All hard and soft landscape works shall be carried out in accordance with the approved details, and to a reasonable standard in accordance with the relevant recommendation of the appropriate British Standard or other recognised code of good practice. The works shall be carried out prior to the occupation of any part of the development or in accordance with the programme agreed by the local planning authority in writing. The maintenance shall be carried out in accordance with the approved schedule. Any trees or plants that, within a period of five years after planting, are removed, die or become in the opinion of the local planning authority, seriously damaged or defective, shall be replaced as soon as is reasonably practicable with others of species, size and number as originally approved, unless the local planning authority gives its written consent to any variation.

Reason: To ensure provision, establishment and maintenance of a reasonable standard of landscaping in accordance with the approved design. (Cambridge Local Plan 2006 policies 3/4, 3/11 and 3/12)

17. No development shall take place until a schedule of landscape maintenance for a minimum period of five years has been submitted to and approved in writing by the local planning authority. The schedule shall include details of the arrangements for its implementation.

Reason: To ensure that the landscaped areas are maintained in a healthy condition in the interests of visual amenity. (Cambridge Local Plan 2006 policies 3/4, 3/11 and 3/12)

18. No development shall take place until there has been submitted to and approved in writing by the local planning authority a plan indicating the positions, design, materials and type of boundary treatments to be erected. The boundary treatment shall be completed before the use hereby permitted is commenced and retained thereafter unless any variation is agreed in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

Reason: To ensure an appropriate boundary treatment is implemented. (Cambridge Local Plan 2006 policies 3/4, 3/11 and 3/12)

19. No development approved by this permission shall be commenced prior to a contaminated land assessment and associated remedial strategy, being submitted to the LPA and receipt of approval of the document/documents from the LPA. This applies to paragraphs a), b) and c). This is an iterative process and the results of each stage will help decide if the following stage is necessary.

(a) The contaminated land assessment shall include a desk study to be submitted to the LPA for approval. The desk study shall detail the history of the site uses and propose a site investigation strategy based on the relevant information discovered by the desk study. The strategy shall be approved by the LPA prior to investigations commencing on site.

(b) The site investigation, including relevant soil, soil gas, surface and groundwater sampling, shall be carried out by a suitable qualified and accredited consultant/contractor in accordance with a quality assured sampling and analysis methodology.

(c) A site investigation report detailing all investigative works and sampling on site, together with the results of the analysis, risk assessment to any receptors and a proposed remediation strategy shall be submitted to the LPA. The LPA shall approve such remedial works as required prior to any remediation commencing on site. The works shall be of such a nature as to render harmless the identified contamination given the proposed end use of the site and surrounding environment including any controlled waters.

No development approved by this permission shall be occupied prior to the completion of any remedial works and a validation report/s being submitted to the LPA and receipt of approval of the document/documents from the LPA. This applies to paragraphs d), e) and f).

(d) Approved remediation works shall be carried out in full on site under a quality assurance scheme to demonstrate compliance with the proposed methodology and best practice guidance.

(e) If, during the works contamination is encountered which has not previously been identified then the additional contamination shall be fully assessed and an appropriate remediation scheme agreed with the LPA.

(f) Upon completion of the works, this condition shall not be discharged until a closure report has been submitted to and approved by the LPA. The closure report shall include details of the proposed remediation works and quality assurance certificates to show that the works have been carried out in full in accordance with the approved methodology. Details of any post-remedial sampling and analysis to show the site has reached the required clean-up criteria shall be included in the closure report together with the necessary documentation detailing what waste materials have been removed from site.

Reason: In order to ensure that any contamination is identified on the site before the commencement of the scheme and enable any mitigation/remediation measures to be implemented during the development phase.

20. Before starting any brick or stone work, a sample panel of the facing materials to be used shall be erected on site to establish the detail of bonding, coursing and colour and type of jointing and shall be agreed in writing with the local planning authority. The quality of finish and materials incorporated in any approved sample panel(s), which shall not be demolished prior to completion of development, shall be maintained throughout the development.

Reason: In the interests of visual amenity and to ensure that the quality and colour of the detailing of the brickwork/stonework and jointing is acceptable and maintained throughout the development. (Cambridge Local Plan 2006 policies 3/4 and 3/12)

21. No development shall take place until samples of the materials to be used in the construction of the external surfaces of the development hereby permitted have been submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 3/14)

INFORMATIVE: It is an OFFENCE to carry out any works within the public highway, which includes a public right of way, without the permission of the Highway Authority. Please note that it is the applicant's responsibility to ensure that, in addition to planning permission, any necessary consents or approvals under the Highways Act 1980 and the New Roads and Street Works Act 1991 are also obtained from the County Council.

INFORMATIVE: No part of any structure may overhang or encroach under or upon the public highway unless licensed by the Highway Authority and no gate / door / ground floor window shall open outwards over the public highway. Public Utility apparatus may be affected by this proposal. Contact the appropriate utility service to reach agreement on any necessary alterations, the cost of which must be borne by the applicant.

INFORMATIVE: To satisfy the condition requiring the submission of a program of measures to control airborne dust above, the applicant should have regard to the Council's Supplementary Planning Document - "Sustainable Design and Construction 2007", and the "Control of dust and emissions from construction and demolition" - Best Practice Guidance produced by the London Councils.

INFORMATIVE: New development can sometimes cause inconvenience, disturbance and disruption to local residents, businesses and passers by. As a result the City Council runs a Considerate Contractor Scheme aimed at promoting high standards of care during construction. The City Council encourages the developer of the site, through its building contractor, to join the scheme and agree to comply with the model Code of Good Practice, in the interests of good neighbourliness. Information about the scheme can be obtained from the Construction Monitoring Officer in the Planning Department (Tel: 01223 457121).

Reasons for Approval

1. This development has been approved subject to conditions and the prior completion of a section 106 planning obligation (/a unilateral undertaking), because subject to those requirements it is considered to conform to the Development Plan as a whole, particularly the following policies:

Cambridgeshire and Peterborough Structure Plan 2003: SS1 and ENV7

Cambridge Local Plan (2006): 3/4, 3/10, 3/12 and 5/1

2. The decision has been made having had regard to all other material planning considerations, none of which was considered to have been of such significance as to justify doing other than grant planning permission.

3. In reaching this decision the local planning authority has acted on guidance provided by the National Planning Policy Framework, specifically paragraphs 186 and 187. The local planning authority has worked proactively with the applicant to bring forward a high quality development that will improve the economic, social and environmental conditions of the area.

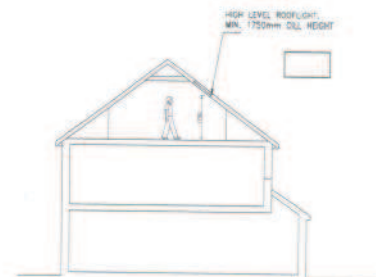
These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.



SECOND FLOOR



ROOF PLAN



SECTION

MATERIALS.

WALLS:
BROWN FACING BRICKWORK.

ROOF:
RED PLAIN TILES.

WINDOWS:
PAINTED TIMBER



FIRST FLOOR



REAR ELEVATION



SIDE ELEVATION



GROUND FLOOR



FRONT ELEVATION



SIDE ELEVATION

TYPE C
4 BED + STUDY
1800 SQ FT



REV P4 15-01-13
GARAGE SIZE'S AMENDED TO URBAN
DESIGN COMMENTS
REV P3 11-10-12
GARAGE WIDENED
REV P4 18-11-12
CHIMNEY AND BWP ADDED WINDOW
REMOVED FROM SIDE ELEVATION.
REV P5 23-01-13
CHIMNEY LOCATION AMENDED

CLIENT

Laragh House Developments Ltd./
Enterprise Property Group Ltd.

PROJECT

Redevelopment of land r/o
231-249 Milton Road,
Cambridge

TITLE

HOUSE TYPE C

DRAWING STATUS

PLANNING

DRAWN: CHECKED:

SCALE: 1:100 @ A1

DATE: June 2012

THE PRACTICE
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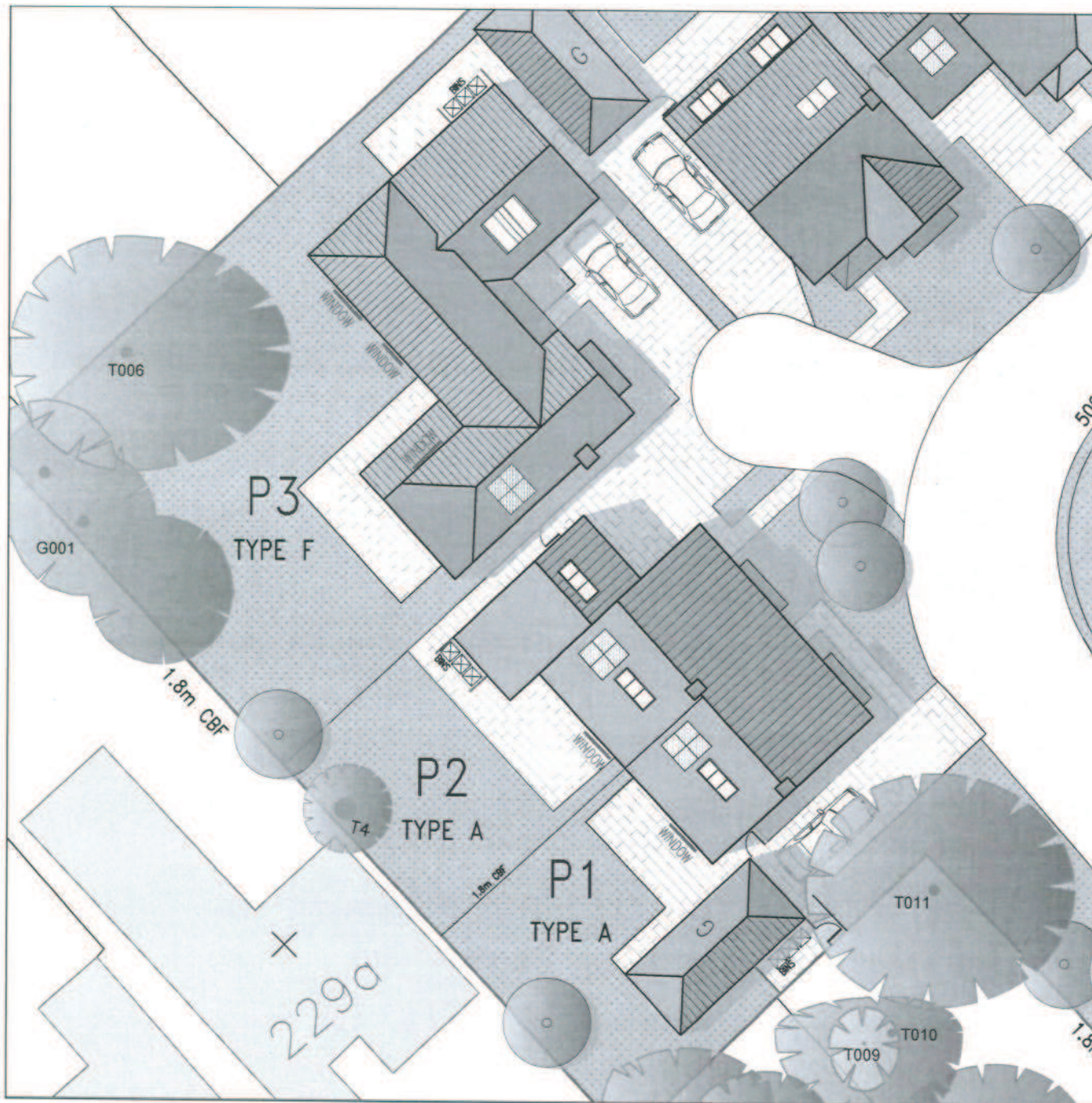
JOB NO. 047 DRAWING NUMBER P-04 REV P5

SITE LEGENDS-

	EXISTING TREE LOCATIONS AS PER LATEST TREE SURVEY.
	PROPOSED TREE LOCATIONS TO LANDSCAPE ARCHITECT DRAWINGS.
	CLOSED BOARDED FENCE
	BLOCK PAVING.
	ROOF INTEGRATED SOLAR PANELS
	BIN STORE WITH TRELLIS FENCE SURROUND.

PROJECT
Redevelopment of land s/o
231-249 Milton Road,
Cambridge








DRAWING STATUS:	
PLANNING	
DRAWN	CHECKED
SCALE	1:250 @ A1
DATE	JULY 12



EXTRACT FROM DRAWING
047 P-01 TO CLEARLY INDICATE
FIRST FLOOR CLEAR GLAZED
WINDOWS OF PLOTS 1-3.

Scale 1:250

SITE LEGENDS-

-  EXISTING TREE LOCATIONS AS PER LATEST TREE SURVEY.
-  PROPOSED TREE LOCATIONS TO LANDSCAPE ARCHITECT DRAWINGS.
- CBF CLOSED BOARDED FENCE.
-  BLOCK PAVING.
-  ROOF INTEGRATED SOLAR PANELS
-  BIN STORE WITH TRELLIS FENCE SURROUND.
-  FIRST FLOOR CLEAR GLAZED WINDOW LOCATION.
-  HIGH LEVEL ROOFLIGHTS.

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architecture

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PLANNING COMMITTEE

Date: 6th March 2013

Application Number	13/0034/REM	Agenda Item	
Date Received	11th January 2013	Officer	Mr John Evans
Target Date	12th April 2013		
Ward	Newnham		
Site	West Cambridge Site Madingley Road Cambridge Cambridgeshire		
Proposal Applicant	Proposed new building for University Data Centre. Chancellor Masters And Scholars University Of Cambridge C/o Agent		

SUMMARY	<p>The development accords with the Development Plan for the following reasons:</p> <ol style="list-style-type: none"> 1. The proposed new data centre is compliant with the Masterplan design criteria. 2. The building will provide a centralised data centre so that energy and infrastructure can be shared in an efficient way. 3. The visual impact of the building has been adequately tested and the external materials will give a high quality appearance.
RECOMMENDATION	APPROVAL

1.0 SITE DESCRIPTION/AREA CONTEXT

- 1.1 The West Cambridge Site is a major academic development undertaken by the University of Cambridge, allocated as Site 7.06 (policy 7/6) in the Cambridge Local Plan (2006). The 66.5 hectare site allocated for higher education provides B1b) uses, sui generis research institutes, staff and student housing, sports and other shared facilities. The site gained outline planning

approval for a Masterplan granted in 1999, which dictates the uses and floorspace of those uses within each of the individual plots on the site. In 2004 the Local Planning Authority approved changes in a revision of the original Masterplan.

- 1.2 The campus is bounded by Madingley Road to the north, the M11 to the west, residential properties to the east, and greenbelt land beyond the Coton footpath to the south. The application site is within the south west corner of the campus and falls within plot J, which is at present undeveloped.
- 1.3 The site does not fall within a Conservation Area. To the immediate west of the site is a triangular shaped area of planting which is a City Wildlife Site.

2.0 THE PROPOSAL

- 2.1 Permission is sought for the erection of a new building to accommodate a proposed data centre. The data centre is intended to provide a centralised computing service for the University, in the interests of ease of maintenance, servicing and efficiency.
- 2.2 The development is a functional building, containing data halls, power and cooling plant and ancillary circulation space. It has a rectangular shaped footprint and stands 11.7m in height. The building will be clad in matt black metal cladding with a large section of 'living wall' to the north and east elevation.
- 2.3 To the west of the building is a rear service plant area, which is screened from the north by vertical coloured cladding.
- 2.4 The building will be sited within a landscaped plot behind 2.4m high steel palisade fencing with landscaped sections.
- 2.5 The application is accompanied by the following supporting information:
 1. Design and Access Statement
 2. Energy and Renewable Strategy
 3. Flood Risk Assessment
 4. Ecological Statement
 5. Archaeological Statement
 6. Noise Assessment

3.0 SITE HISTORY

Reference	Description	Outcome
97/0961/OP	Outline application for the development of 66.45ha of land for University academic departments (73,000sq.m), research institutes (24,000sq.m), commercial research (41,000sq.m) and associated infrastructure	Approved
10/0315/REM	Phase 3 infrastructure works consisting of new access arrangements (extension of Charles Babbage Road, realignment of access road A), car parking, new pedestrian/cycle routes, west square and forum, western balancing lake, and associated hard and soft landscaping.	Approved
12/1391/REM	Extension of internal road network (access road); extension of pedestrian and cycle routes, associated hard and soft landscaping (an environmental statement was submitted at the time of the outline application).	Approved

4.0 PUBLICITY

4.1	Advertisement:	Yes
	Adjoining Owners:	Yes
	Site Notice Displayed:	Yes

5.0 POLICY

- 5.1 See Appendix 1 for full details of Central Government Guidance, East of England Plan 2008 policies, Cambridgeshire and Peterborough Structure Plan 2003 policies, Cambridge Local Plan 2006 policies, Supplementary Planning Documents and Material Considerations.

5.2 Relevant Development Plan policies

PLAN	POLICY NUMBER
Cambridge Local Plan 2006	3/4 3/6 3/7 3/11 3/12 4/1 4/13 7/6 8/2 8/6 8/16 10/1

5.3 Relevant Central Government Guidance, Supplementary Planning Documents and Material Considerations

Central Government Guidance	National Planning Policy Framework March 2012 Circular 11/95 Community Infrastructure Levy Regulations 2010
Supplementary Planning Documents	Sustainable Design and Construction Waste Management Design Guide
Material Considerations	<u>Central Government:</u> Letter from Secretary of State for Communities and Local Government (27 May 2010) Written Ministerial Statement: Planning for Growth (23 March 2011)
	<u>Citywide:</u> Arboricultural Strategy Cambridgeshire Quality Charter for Growth

6.0 CONSULTATIONS

Cambridgeshire County Council (Engineering)

- 6.1 Provided that the application accords with the Masterplan for the overall development the Highway Authority have no additional comment to make.

Head of Environmental Services

- 6.2 No objections, subject to ground contamination and noise related conditions.

Urban Design and Conservation Team

- 6.3 Page 60 of the West Cambridge Masterplan (1999) states building heights within Plot J should 'generally be between 8.5m and 12m above finished ground level, which corresponds with 2 to 3 storeys for research building'.

Following concerns raised at pre-application discussions, the proposal has been reduced in height from 14m to 12m, as a result of externalising much of the plant uses previously proposed within the roof space. The proposal includes a sloping mono-pitch roof, the lower part of which retains the level parapet around the top of the building to form a visual screen to the roof mounted plant area.

We support the proposed scale of development, which accords with the maximum height parameters for Plot J as outlined within the West Cambridge Masterplan (1999). The addition of the parapet will conceal any roof-mounted plant.

Senior Sustainability Officer (Design and Construction)

- 6.4 The approach that is being taken to sustainable design and construction and carbon reduction is supported. While the proposal does not meet the requirements of Policy 8/16, given the specific nature of this building and its significant process loads, it is agreed that meeting this policy would be very difficult. The proposals provide an opportunity to deliver a purpose built energy efficient data centre, consolidating the University's existing data facilities, bringing about a 40%

reduction in carbon emissions when compared to the existing situation. This approach is supported, although some clarification as to the future proofing of the electrical services would be welcomed.

Head of Streets and Open Spaces (Landscape Team)

- 6.5 The Landscape Team has reviewed the application and support the development.

The proposed native scrub planting will aid the screening / softening process in time, however we would like to see some larger stock sizes (such as birch and rowan) within this mix to provide instant screening as well as providing a shelter for eventual larger forest scale trees such as the oak.

The mix should consist of whips and larger stock sizes as mentioned above, at 12-14cm girth. The mix also requires another larger species as oak is a very slow grower. Please add Lime to the mix, these should be at the larger stock size.

We do not support green walls mainly because of the extremely heavy maintenance burden in terms of watering and nutrient supply as well as the short life span of the plants.

Design and Conservation Panel (Meeting of 24 October 2012)

- 6.6 The Panel's comments are as follows:

- ☐ The Panel note that the West Cambridge masterplanners MJP have been consulted with during the development of the scheme.
- ☐ 'Living wall'. This is welcomed although is highly specialised and costly. Providing sufficient growing space is provided at ground level, the Panel would suggest the use of climbers as a more cost-effective and lower maintenance alternative – potentially allowing greater resources to be directed at a good quality general cladding material/detailing. Should the final scheme include a genuine living wall, the Panel would welcome additional measures to encourage bird life such as kestrel boxes.

- External escape staircase. The Panel would welcome a cladding solution but are aware that the current arrangement is temporary in light of possible future expansion.
- Materials (corrugated steel cladding). The Panel questioned the choice of what they saw as crude, inexpensive materials usually with highly visible fixings and poor quality window detailing. Alternatives should be explored.
- Roof. Some doubt was expressed as to the capability of the guttering to deliver an effective and substantially maintenance-free drainage system on a mono-pitched roof to a corrugated steel clad building. This is a significant concern given the building's function and the vulnerability of the equipment and services within.
- Hard landscaping (spur road and parking to the east of the building). The Panel felt the emphasis on hard surfacing was too reminiscent of industrial estates and should be re-examined.
- Front door canopy and elevational composition. The use of brown in contrast to the black and green was seen as too industrial. This should be removed for greater simplification.
- Sustainability measures. This is a passive building that uses and generates huge amounts of energy. Measures such as load sharing systems and the possibility of using the building to host other energy generating infrastructure (solar panels on the roof for instance) could be examined.
- Fencing. The Panel accept the need for the site to be secure. However, whilst the proposed articulated metal fencing was elegant in design, doubt was expressed about its effectiveness and appropriateness, given the broader masterplan and building concept. The Panel would welcome consideration of alternatives with a landform/landscaping component on the site's perimeter (with a fence set back perhaps) which would not mask the simplicity and inscrutability of the building's form and its setting.
- The possibility of a pedestrian and cycle path connection to the Coton footbridge was queried.
- Lighting strategy. Details of the external illumination of the building were not available to the Panel. The Panel would hope that any such lighting should not be excessive and be respectful to the site's position on the edge of West Cambridge.

Conclusion

The Panel were comfortable with the form and scale of the proposed building and the strategy for the future development

of the site. The panel also appreciated the initial concept – as communicated by the simple sketch drawing included within the presentation - but felt that this hadn't been carried through to the detail of the proposal. In particular, given the choice of cladding material, the building's success in use will be partly dependant on the detailing of the building which it was felt should be simplified. Securing the perimeter of the site, possibly through the use of innovative landscaped features, would add to the scheme's assimilation in its setting.

VERDICT – AMBER (9) and GREEN (1) providing the concerns on detailing are addressed.

Design and Conservation Panel (Meeting of 12 December 2012)

6.7 The conclusions of the Panel meeting were as follows:

- ☐ The Panel welcomed the design team's comprehensive response to the issues raised at the last submission and the quality of the presentation material.
- ☐ Orange palisade fence. Although this is not visible in most of the views presented, the Panel appreciate the architects' desire for contrast but questioned the need to make this contrast so pointed.
- ☐ Cladding. The Panel support the use of a matt cladding as this is likely to be more resistant to weathering.
- ☐ Landscaping. A re-examination of the finer landscaping details would be welcomed, particularly the handling of the edges.
- ☐ Living wall. The concerns raised last time remain, as living walls are costly to maintain and not always successful.
- ☐ Sustainability. The Panel expressed some disappointment that further detail was not provided on the building's ability to provide renewable energy and hoped that consideration would be given to the use elsewhere of the residual heat generated by the project.

Conclusion

The Panel welcome the positive response to its concerns. Only a few issues remain. The Panel does wish to emphasise the importance of high quality detailing to ensure that the building meets the aspirations of both the architects and the University. The Panel thought this particularly important when considering

the detailing of the cladding, as the adoption of a 'light touch' approach would be disappointing.

VERDICT – GREEN (unanimous)

The above responses are a summary of the comments that have been received. Full details of the consultation responses can be inspected on the application file.

Access Officer

No objections.

7.0 REPRESENTATIONS

7.1 The owners/occupiers of the following addresses have made representations:

75 Gough Way,

7.2 The representations can be summarised as follows:

- This building will sit adjacent to the Coton Footpath, which is a part of the Harcamlow Way.
- The first building one sees as one enters Cambridge will be the data centre. The view will be dreadful.
- The building has no architectural merit and is a missed opportunity.
- A question is raised regarding environmental standards.

7.3 The above representations are a summary of the comments that have been received. Full details of the representations can be inspected on the application file.

8.0 ASSESSMENT

8.1 From the consultation responses and representations received and from my inspection of the site and the surroundings, I consider that the main issues are:

1. Principle of development
2. Context of site, design and external spaces
3. Renewable energy and sustainability
4. Residential amenity

5. Refuse arrangements
6. Car and cycle parking
7. Disabled access
8. Third party representations
9. Planning Obligation Strategy

Principle of Development

- 8.2 Local Plan Policy 7/6 states that development for University needs will be permitted on the West Cambridge Site, South of Madingley Road. The site provides development opportunity during the plan period and beyond. Further development which accords with the provisions of the Masterplan will be permitted.
- 8.3 This is a reserved matters application pursuant to the previously approved Masterplan, approved in outline form in 1999, and follows earlier phases of infrastructure reserved matters. The outline consent is material to the determination of this application.
- 8.4 The development is in accordance with the guidelines for plot J in the West Cambridge Masterplan. The plot is designated mainly for use by Research Institutes, with smaller areas for academic use and some commercial research. The design guidelines also specifically mentions the relocation of University stores. While a data centre was not envisaged at the time of the Masterplan, the proposed use falls within the scope of uses intended for Plot J. In quantitative terms, the floorspace of the building is broadly consistent with the proposed plot ratio set out for Plot J.
- 8.5 In light of the above, subject to the evaluation of other issues below, I consider the principle of development accords with both the Masterplan and with Cambridge Local Plan policy 7/6.

Context of site, design and external spaces

- 8.6 The key design issue is the design and appearance of the new building within its setting and in relation to the masterplan guidelines.

Layout, Scale and Massing

- 8.7 The form, layout, size and scale of the proposed data centre broadly accords with the design guidelines of the Masterplan. The nature of use of the building means that it cannot have an active, public frontage, but it does address the new street to the east in manner which was envisaged within the Masterplan. This is through its entrance and site layout. Opportunities for natural surveillance are limited due to the functional nature of the building, but the proposed staff areas provide views towards the site entrance and north towards future new buildings within plot J which maximizes potential surveillance.
- 8.8 The height parameters for plot J are between 8.5m and 12m, which corresponds to a two or three storey building. The proposed data centre is under 12m in height, which is achieved by housing most of the plant equipment outside of the building. The rooftop has a sloping monopitch and a parapet around the edge which obscures roof top plant. The scale and massing is therefore in accordance with the Masterplan guidelines and is supported by the Design and Conservation Panel and the Urban Design and Conservation Team.
- 8.9 I recognise that the building will be relatively prominent from the Coton footpath and within the immediate environs in the south west corner of the campus. This is because the building is the first to be developed within plot J. The University has visually tested the visual impact on the building from significant viewpoints both within and outside of the campus. I am satisfied that with appropriate landscaping the building will not appear unduly conspicuous in its setting and when viewed from the relatively higher ground beyond the motorway to the west.
- 8.10 The siting of the building could accommodate possible future extensions should the storage needs for the data centre change in the future.

Detailed Design and Materials

- 8.11 The building is contemporary in design and appearance with minimalist detailing, and the use of matt black cladding panels. Following further explanation of the materials palette at the second meeting of the Design and Conservation Panel, there is

broad support for this approach. Final materials can be secured by condition 2.

- 8.12 Some concerns have been raised regarding the maintenance, cost and overall success of the proposed green wall. While I recognise that there are shortcomings with this design feature, the benefits to visual amenity in my view outweigh the maintenance burden. The irrigation, planting structure and maintenance regime can be adequately controlled through the suggested landscape conditions. The green wall is an integral part of the design and the University understands the maintenance commitment associated with a green wall.

External Spaces and Landscaping

- 8.13 The Landscape Team have considered this scheme and are broadly supportive of the approach taken to the landscaped setting of the building. Several detailed suggestions as to the species to aid screening have been recommended, which can be agreed through the imposition of landscape conditions 10, 11, 12 and 13.
- 8.14 Given the sensitive nature of the use of the building, the palisade steel fencing is essential for site security. As raised by the Design and Conservation Panel, the appearance of the fencing will be softened with rectangular planted sections, which have been increased in number from the original submission and will help relieve its visual impact.
- 8.15 The rear plant compound is screened from the public domain and will not in my view detract from the minimalist design of the building. Final details of the coloured plant screen cladding can be ensured through the discharge of the material samples condition.
- 8.16 In my opinion the proposal is compliant with the Masterplan design guidelines and with Cambridge Local Plan (2006) policies 3/4, 3/7, 3/11 and 3/12.

Renewable energy and sustainability

- 8.17 The Council's Sustainability Officer has commented on this scheme. It is recognised that for a building of this type, with significant electrical process loads, achievement of 10% carbon

reduction required by Local Plan 8/16 would be extremely difficult. A broader carbon reduction approach has been proposed which is supported. Taking into account the consolidation of the University's existing data centre facilities, the scheme as a whole would bring about a 40% reduction in carbon emissions as compared with the existing situation.

8.18 The building has also been designed to have the ability to connect to any future district heat energy network proposed for West Cambridge.

8.19 In my opinion the applicants have suitably addressed the issue of sustainability and renewable energy and the proposal is in accordance with Cambridge Local Plan (2006) policy 8/16 and the Sustainable Design and Construction SPD 2007.

Residential Amenity

8.20 The development will not impact on any residential properties because of its isolated situation within the south west of the campus.

Refuse Arrangements

8.21 A refuse and recycling area is incorporated within the rear service compound. Environmental Health request further details of the circulation route which can be secured by condition 9. In my opinion the proposal is compliant with Cambridge Local Plan (2006) policy 3/12.

Car and Cycle Parking

Car Parking

8.22 The data centre provides 5 car parking spaces, which is considered appropriate for the nature of the use of the building. Car parking provision has been agreed campus wide through the masterplan and the site benefits from access to a bus link.

Cycle Parking

8.23 The site provides cycle parking for 8 bicycles which is considered proportionate for the use of the building. In my

opinion the proposal is compliant with Cambridge Local Plan (2006) policies 8/6 and 8/10.

Disabled access

- 8.24 The building will have a limited number of staff and visitors; the Council's Access Officer is nevertheless satisfied that it is convenient for disabled people. In my opinion the proposal is compliant with Cambridge Local Plan (2006) policies 3/7 and 3/12.

Third Party Representations

- 8.25 The issues raised in the representation received have been covered in the above report. Please see paragraph 8.9.

Planning Obligations

- 8.26 Planning Obligations are covered by the S106 Agreement for the outline consent.

9.0 CONCLUSION

- 9.1 The proposed data centre will be a provide a high quality contemporary building, providing a reduction in the University's carbon emissions through the merging of other data centre across the City. The design is compliant with the Masterplan criteria, and in combination with the package of landscaping, will make a positive contribution to the appearance of the growing campus. APPROVAL is recommended.

10.0 RECOMMENDATION

APPROVE subject to the following conditions and reasons for approval:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In accordance with the requirements of section 51 of the Planning and Compulsory Purchase Act 2004.

2. No development shall take place until samples of the materials to be used in the construction of the external surfaces of the development hereby permitted have been submitted to and approved in writing by the local planning authority. Development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the external surfaces is appropriate. (Cambridge Local Plan 2006 policies 3/4, 3/12 and 3/12)

3. Except with the prior written agreement of the local planning authority no construction work or demolition shall be carried out or plant operated other than between the following hours: 0800 hours to 1800 hours Monday to Friday, 0800 hours to 1300 hours on Saturday and at no time on Sundays, Bank or Public Holidays.

Reason: To protect the amenity of the adjoining properties. (Cambridge Local Plan 2006 policy 4/13)

4. No development approved by this permission shall be commenced prior to a contaminated land assessment and associated remedial strategy, together with a timetable of works, being submitted to the LPA for approval.

(a)The contaminated land assessment shall include a desk study to be submitted to the LPA for approval. The desk study shall detail the history of the site uses and propose a site investigation strategy based on the relevant information discovered by the desk study. The strategy shall be approved by the LPA prior to investigations commencing on site.

(b)The site investigation, including relevant soil, soil gas, surface and groundwater sampling, shall be carried out by a suitable qualified and accredited consultant/contractor in accordance with a quality assured sampling and analysis methodology.

(c) A site investigation report detailing all investigative works and sampling on site, together with the results of the analysis, risk assessment to any receptors and a proposed remediation strategy shall be submitted to the LPA. The LPA shall approve such remedial works as required prior to any remediation commencing on site. The works shall be of such a nature as to render harmless the identified contamination given the proposed end use of the site and surrounding environment including any controlled waters.

(d) Approved remediation works shall be carried out in full on site under a quality assurance scheme to demonstrate compliance with the proposed methodology and best practice guidance.

(e) If, during the works contamination is encountered which has not previously been identified then the additional contamination shall be fully assessed and an appropriate remediation scheme agreed with the LPA.

(f) Upon completion of the works, this condition shall not be discharged until a closure report has been submitted to and approved by the LPA. The closure report shall include details of the proposed remediation works and quality assurance certificates to show that the works have been carried out in full in accordance with the approved methodology. Details of any post-remedial sampling and analysis to show the site has reached the required clean-up criteria shall be included in the closure report together with the necessary documentation detailing what waste materials have been removed from site.

Reason: In the interests of amenity of future users of the site, Cambridge Local Plan 2006 policy 4/13.

5. Prior to the commencement of the development hereby approved (including any pre-construction, demolition, enabling works or piling), the applicant shall submit a report in writing, regarding the demolition / construction noise and vibration impact associated with this development, for approval by the local authority. The report shall be in accordance with the provisions of BS 5228-1:2009 Code of Practice for noise and vibration control on construction and open sites and include full details of any piling and mitigation measures to be taken to protect local residents from noise and or vibration. Development shall be carried out in accordance with the approved details.

Due to the proximity of this site to existing residential premises and other noise sensitive premises, impact pile driving is not recommended.

Reason: To protect the amenity of nearby properties, Cambridge Local Plan 2006 policy 4/13.

6. Except with the prior agreement of the local planning authority in writing, there should be no collection or deliveries to the site during the demolition and construction stages outside the hours of 0700 hrs and 1900 hrs on Monday ' Saturday and there should be no collections or deliveries on Sundays or Bank and public holidays.

Reason: To protect the amenity of nearby properties, Cambridge Local Plan 2006 policy 4/13.

7. Before the development/use hereby permitted is commenced, a scheme for the insulation of the building(s) and/or plant in order to minimise the level of noise emanating from the said building(s) and/or plant shall be submitted to and approved in writing by the local planning authority and the scheme as approved shall be fully implemented before the use permitted is commenced.

To protect the amenity of the adjoining properties. (Cambridge Local Plan 2006 policy 4/13).

8. Before the development/use hereby permitted is occupied, a scheme for the insulation of the building(s) and/or emergency generator in order to minimise the level of noise emanating from the said building(s) and/or emergency generator shall be submitted to and approved in writing by the local planning authority and the scheme as approved shall be fully implemented before the use hereby permitted is commenced. The scheme shall include the following:

(i) Generator - Emergency Use Only

The generator shall only be used in the event of mains power failure or in accordance with (ii) below. It shall not be used as an alternative supply in the event of disconnection from the mains supply following for example non-payment.

(ii) Generator - Hours of Running for Maintenance

Running of the generator as part of routine maintenance and repair shall only take place, for a period of time to be agreed, between the hours of 8am ' 6pm Monday to Friday, 9am '1pm Saturday and no time Sunday or Public Holidays.

To protect the amenity of the adjoining properties. (Cambridge Local Plan 2006 policy 4/13).

9. No development shall commence until further details of the circulation route for refuse collection vehicles have been submitted to the local planning authority and approved in writing. The required details shall include a full construction specification for the route, and a plan defining the extent of the area to which that specification will be applied. The development shall not be occupied until the refuse vehicle circulation route has been laid out and constructed in accordance with the details thus approved, and thereafter the route shall be maintained in accordance with those details.

Reason: In order that satisfactory arrangements are in place for waste, Cambridge Local Plan 2006 policy 3/12.

10. Notwithstanding the details submitted in support of the application, prior to occupation of the building hereby approved full details of both hard and soft landscape works, including the green wall feature, have been submitted to and approved in writing by the local planning authority and these works shall be carried out as approved. These details shall include proposed finished levels or contours; means of enclosure; car parking layouts, other vehicle and pedestrian access and circulation areas; hard surfacing materials; minor artefacts and structures (eg furniture, play equipment, refuse or other storage units, signs, lighting); proposed and existing functional services above and below ground (eg drainage, power, communications cables, pipelines indicating lines, manholes, supports); retained historic landscape features and proposals for restoration, where relevant. Soft Landscape works shall include planting plans; written specifications (including cultivation and other operations associated with plant and grass establishment); schedules of plants, noting species, plant sizes and proposed numbers/densities where appropriate and an implementation programme.

Reason: In the interests of visual amenity and to ensure that suitable hard and soft landscape is provided as part of the development. (Cambridge Local Plan 2006 policies 3/4, 3/11 and 3/12)

11. Prior to occupation of the building hereby approved a schedule of landscape maintenance for a minimum period of five years has been submitted to and approved in writing by the local planning authority. The schedule shall include details of the arrangements for its implementation.

Reason: To ensure that the landscaped areas are maintained in a healthy condition in the interests of visual amenity. (Cambridge Local Plan 2006 policies 3/4, 3/11 and 3/12)

12. A landscape management plan, including long term design objectives, management responsibilities and maintenance schedules for all landscape areas, shall be submitted to and approved by the local planning authority in writing prior to occupation of the development for its permitted use. The landscape plan shall be carried out as approved.

Reason: In the interests of visual amenity and to ensure that suitable hard and soft landscape is provided as part of the development. (Cambridge Local Plan 2006 policies 3/4, 3/11 and 3/12)

13. All hard and soft landscape works shall be carried out in accordance with the approved details, and to a reasonable standard in accordance with the relevant recommendation of the appropriate British Standard or other recognised code of good practice. The works shall be carried out prior to the occupation of any part of the development or in accordance with the programme agreed by the local planning authority in writing. The maintenance shall be carried out in accordance with the approved schedule. Any trees or plants that, within a period of five years after planting, are removed, die or become in the opinion of the local planning authority, seriously damaged or defective, shall be replaced as soon as is reasonably practicable with others of species, size and number as originally approved, unless the local planning authority gives its written consent to any variation.

Reason: To ensure provision, establishment and maintenance of a reasonable standard of landscaping in accordance with the approved design. (Cambridge Local Plan 2006 policies 3/4, 3/11 and 3/12).

INFORMATIVE: New development can sometimes cause inconvenience, disturbance and disruption to local residents, businesses and passers by. As a result the City Council runs a Considerate Contractor Scheme aimed at promoting high standards of care during construction. The City Council encourages the developer of the site, through its building contractor, to join the scheme and agree to comply with the model Code of Good Practice, in the interests of good neighbourliness. Information about the scheme can be obtained from The Considerate Contractor Project Officer in the Planning Department (Tel: 01223 457121).

Reasons for Approval

1. This development has been approved, conditionally, because subject to those requirements it is considered to conform to the Development Plan as a whole, particularly the following policies:

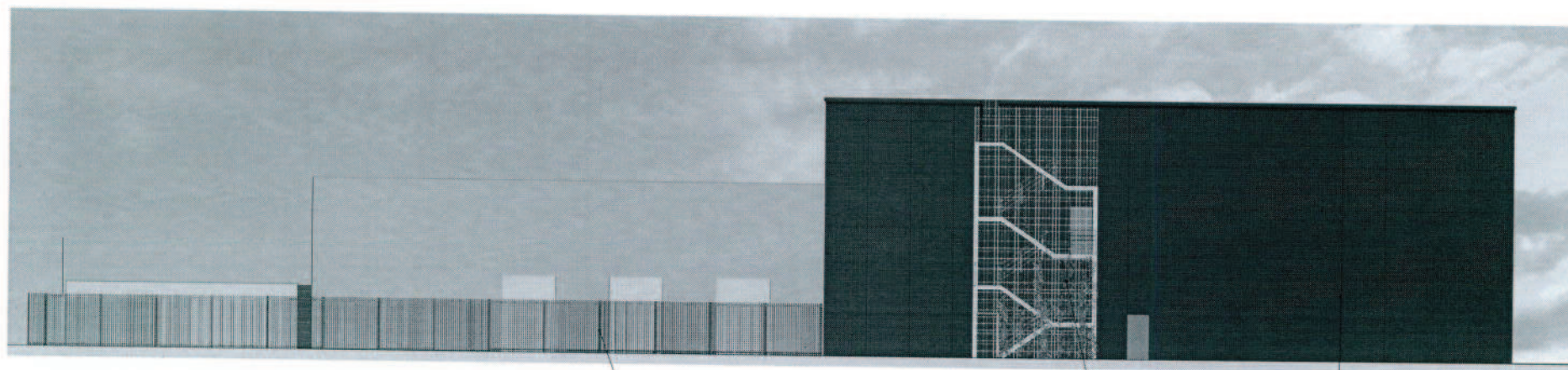
Cambridge Local Plan (2006): 3/4, 3/6, 3/7, 3/11, 3/12, 4/1, 4/13, 4/16, 8/2, 8/6, 8/16, 10/1.

2. The decision has been made having had regard to all other material planning considerations, none of which was considered to have been of such significance as to justify doing other than grant planning permission.

3. In reaching this decision the local planning authority has acted on guidance provided by the National Planning Policy Framework, specifically paragraphs 186 and 187. The local planning authority has worked proactively with the applicant to bring forward a high quality development that will improve the economic, social and environmental conditions of the area.

These reasons for approval can be a summary of the reasons for grant of planning permission only. For further details on the decision please see the officer report online at www.cambridge.gov.uk/planningpublicaccess or visit our Customer Service Centre, Mandela House, 4 Regent Street, Cambridge, CB2 1BY between 8am to 6pm Monday to Friday.

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SOUTH ELEVATION

Steel palisade fence 2.4m high

Escape staircase

Black metal cladding



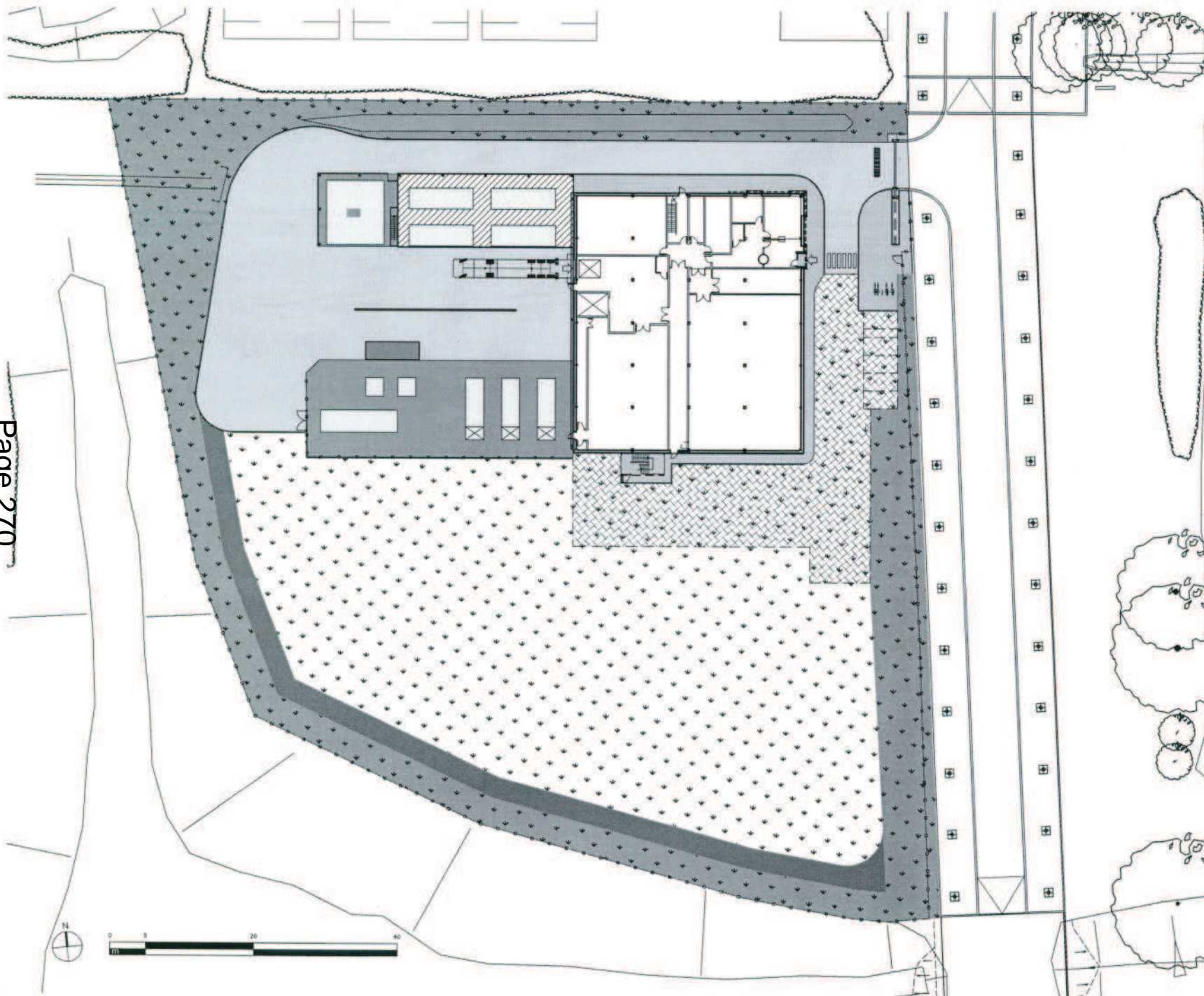
WEST ELEVATION















Loading bay door

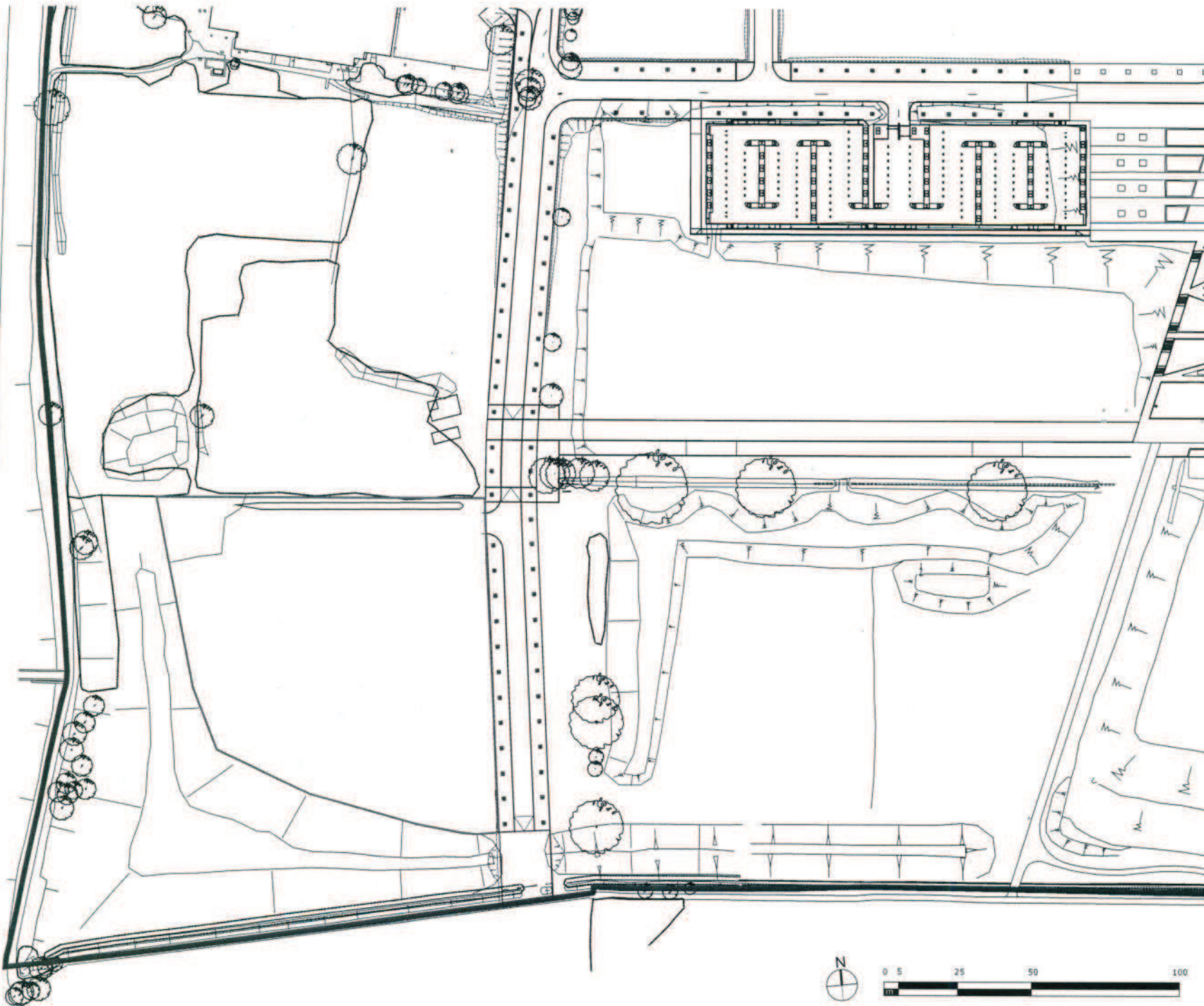
Black metal cladding

Escape staircase

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KEY:			
	STEEL PALISADE TO BOUNDARY FENCE		
	ACCESS ROAD, PARKING & SERVICE YARD		
	PEDESTRIAN PAVING		
	EXTERNAL PLANT COMPOUNDS: Fenced enclosures on hardstanding with gates or removable panels, where required, for access.		
	EXTERNAL PLANT GANTRY FOR COOL TOWERS: with 1100mm high guarding to exposed edges.		
	NATIVE SPECIES SCRUBS		
	RUDERAL MEADOW		
	MEADOW		
	REINFORCED MEADOW		
	SWALE		
	ROAD BLOCKER		
	ACCESS CONTROL POINT		
	REFUSE STORAGE		
	4 BICYCLE HOOPS		
Rev	Date	Revisions	By Ch'd
Drawing Status			
FOR PLANNING			
Project Title			
University of Cambridge West Cambridge Data Centre			
Drawing title			
Site layout			
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University of Cambridge West Cambridge Data Centre			
Drawing title			
Site Location Plan			
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CAMBRIDGE CITY COUNCIL

REPORT OF: Head of Planning
TO: Planning Committee
WARD: Market

Request for Amendment to the Original Officer Recommendation for EC Language School, Gibson House, 57-61 Burleigh Street, Cambridge.

1. INTRODUCTION

- 1.1 On 19 September 2012 Planning Committee considered an application (11/1442/FUL) to allow the change of use of Gibson House, 57-61 Burleigh Street on the upper three floors only from office use B1(a) to office and teaching (B1(a) and D1) in order to allow the applicant, EC Language School to occupy the premises.
- 1.2 Within the planning officer's report presented at the September Committee, paragraphs 8.49 and 8.50 recommended that the S106 Agreement would need to address two elements that had been identified in the assessment of the application. These were the capping of student weeks across the two retained sites, Guildhall Chambers and Gibson House and that the applicant must relinquish its lease of 26-29 Sidney Street. The Committee resolved to approve the officer's recommendation to grant planning permission for the application subject to completion of an S106 Agreement containing the obligations noted within paragraphs 8.49 and 8.50 of the planning officer's report.
- 1.3 Since the Committee meeting the applicant's have advised officers that Sidney Sussex College will not allow them to relinquish their lease. Officers consider that it would not constitute best practice to include an obligation in a S106 Agreement which the applicant has stated it will not be able to comply with. For this reason the application remains undetermined, pending the completion of a S106 Agreement. The application is brought back to Committee for its further consideration.

2. RECOMMENDATIONS

- 2.1 For Members to agree an amendment to the obligations to be secured by the S106 Agreement. Specifically to approve alterations to the originally proposed wording of the S106 Agreement in order to allow the applicant (leaseholder) to sub-let the premises at 26-29 Sidney Street to another office occupier, in accordance with its use class (B1 (a)), until such time that the freeholder, Sidney Sussex College, allows the applicant to relinquish their lease.
- 2.2 There is a further recommendation that Members allow an extension of time for the completion of the S106 Agreement until 30th April 2013.

3. BACKGROUND

- 3.1 Since its meeting on 19th September 2012, officer's have been seeking to secure the legal agreement in the form that Members gave their resolution to agree.
- 3.2 Officers considered that the need to relinquish the lease at 26-29 Sidney Street was appropriate as it ensured that by resolving to approve this application an office use would be allowed to return to the commercial market, where an alternative office occupier would have the opportunity to occupy within central Cambridge, close to amenities and transport links. This is considered necessary to mitigate the impact of the proposed change of use and personal planning permission sought for Guildhall Chambers and Gibson House.
- 3.3 Although the original intention to relinquish the lease is unachievable, the proposal for the applicant to vacate the premises and to sub-let to another office user, still maintains the original planning objectives.

4. CONSULTATIONS

- 4.0 No further consultations have been carried out since the report to Committee on the 19 September 2012 as the planning application has not changed such that further consultation is necessary.

5. OPTIONS

- 5.0 In the absence of agreeing the amendment to the wording, the existing recommendation will prevail and the S106 Agreement will be drafted so as to include the obligation on the applicant to relinquish the lease at 26-29 Sidney Street prior to commencement of use for teaching and office use at Gibson House, 57-61 Burleigh Street. However the expectation is that the obligation would not be capable of being met by the applicant for matters that are outside of their control, and this would result in the applicant continuing to occupy 26-29 Sidney Street and being unable to implement the planning permission for change of use applied for.

6. CONCLUSIONS

- 6.0 I recommend that the Committee approves the recommendation at paragraph 2 of this report.

7. IMPLICATIONS

- (a) **Financial Implications** – None
- (b) **Staffing Implications** – None
- (c) **Equal Opportunities Implications** – None
- (d) **Environmental Implications** – None

Climate Change Impact: Nil

- (e) **Procurement** – None
- (f) **Consultation and Communication** - None
- (g) **Community Safety** - None

BACKGROUND PAPERS: The following are the background papers that were used in the preparation of this report:

Planning application 11/1442/FUL

To inspect these documents contact Sophie Pain on extension 7296

The author and contact officer for queries on the report is Sophie Pain on extension 7296.

Report file:

Date originated:	25 February 2013
Date of last revision:	25 February 2013

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